
Sectional Map Amendment

8

The comprehensive rezoning process, also known in Prince George's County as the sectional map amendment process, allows for the rezoning of a section of the overall county zoning map in order to bring zoning in conformance with approved county plans and policies. This chapter contains the sectional map amendment (SMA) for the Central Annapolis Road Corridor Sector Plan. The SMA implements the land use and urban design recommendations of the approved sector plan.

The District Council initiated the SMA in 2009 through Council Resolution CR-50-2009, with the intent of processing the SMA concurrently with the sector plan. The procedure followed was in accordance with Council Bill CB-39-2005, which amended the framework for the process, whereby the District Council approves the sector plan and SMA simultaneously (originally established in CB-33-1992).

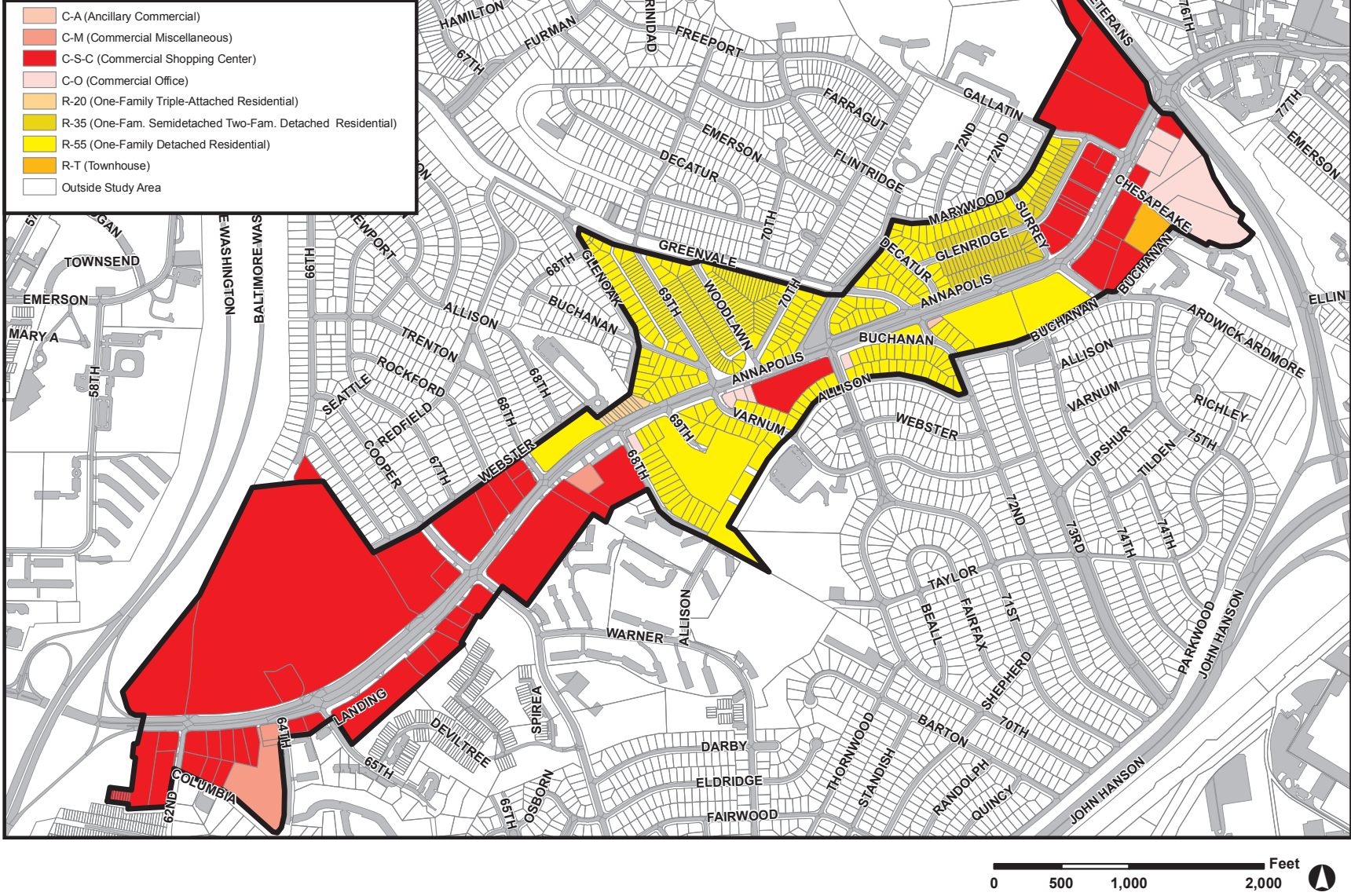
Comprehensive rezoning through the SMA represents an important implementation step in the land use planning process. It ensures that future development will conform to county land use plans and development policies, reflecting the county's ability to accommodate development in the foreseeable future. The SMA process corrects existing zoning that hinders such development, and it reduces piecemeal rezoning. The approval

of the zoning pattern recommended by the sector plan and implemented by this SMA brings zoning into greater conformity with the county land use goals and policies as they apply to the Central Annapolis Corridor, thereby enhancing the health, safety, and general welfare of the all Prince George's County residents and citizens.

The County's Capital Improvement Program and Ten-Year Water and Sewerage Plan, as well as existing land use and zoning and pending zoning applications, were examined and evaluated in preparing both the land use plan and the comprehensive rezoning. Consideration has been given to the environmental and economic impact of the land use and zoning. The approved SMA results in the revision of the official 1"=200' zoning map(s) for this sector plan area. Future comprehensive examinations of the zoning within these areas will occur in accordance with the procedures established for sectional map amendments.

Figure 8.1 on the next page illustrates the existing zoning in the sector plan area.

Figure 8.1 Existing Zoning



Comprehensive Rezoning Implementation Policies

These comprehensive rezoning implementation policies were established by the Prince George's County Planning Board and District Council to guide preparation of the SMA.

Public Policy

The established public land policy states that all land should be placed in the most restrictive and/or dominant adjacent zone, whichever bears the closest relationship to the intended character of the area. Therefore, the rezoning of both public and private land should be compatible with surrounding zones to eliminate any "islands" of inharmonious zoning and still provide for appropriate and preferred land uses. It should further assure compatibility of any future development or uses if the property returns to private ownership.

A distinction is made where large parcels of land are set aside specifically as public open space. In these cases the R-O-S (Reserved Open Space) or O-S (Open Space) Zone is applied as the most appropriate zone, depending on the size of the property.

The comprehensive rezoning process applies a zoning category to all land, including government property, without regard to its unique ownership. Federal and state government property, which is scattered throughout the county, is not subject to the requirements of the Zoning Ordinance. The R-O-S Zone is generally applied to federal and state properties, unless specific uses or intended character of the property or area should warrant another zoning category. This policy complies with Section 27-113 of the Prince George's County Zoning Ordinance, which states that any land that is conveyed in fee simple by the United States of America or by the State of

Maryland shall immediately be placed in the R-O-S Zone until a zoning map amendment for the land has been approved by the District Council.

Zoning in the Public Right-of-Way

Policies governing the zoning of public streets and railroad rights-of way (both existing and proposed) are contained in Section 27-111 of the Prince George's County Zoning Ordinance. The SMA has been prepared in accordance with this section of the ordinance.

Limitations on the Use of Zones

Zoning classifications proposed in an SMA are limited only to the range of zones within the ordinance available at the time of final action by the District Council. However, there are certain restrictions on when these may be applied to properties (Section 27-223) of the Zoning Ordinance.

Reclassification of an existing zone to a less intense zone is prohibited where:

(g)(1) "The property has been rezoned by Zoning Map Amendment within five (5) years prior to the initiation of a Sectional Map Amendment or during the period between initiation and transmittal to the District Council, and the property owner has not consented in writing to such the zoning; or"

(g)(2) "Based on existing physical development at the time of adoption of the Sectional Map Amendment, the rezoning would create a nonconforming use. This rezoning may be approved, however, if there is a significant public benefit to be served by the rezoning based on facts peculiar to the subject property and the immediate neighborhood. In recommending the rezoning, the Planning Board shall identify these properties and provide written justification supporting the rezoning at the time of transmittal. The

failure of either the Planning Board or property owner to identify these properties, or a failure of the Planning Board to provide written justification, shall not invalidate any Council action in the approval of the Sectional Map Amendment.”

In order to clarify the extent to which a given parcel of land is protected from less intensive rezoning by virtue of physical development, the Zoning Ordinance states in Section 27-223(h) that:

“The area of the 'property,' as the word is used in Subsection (g) (2), above, is the minimum required by the Zoning Ordinance which makes the use legally existing when the Sectional Map Amendment is approved.”

Comprehensive rezoning of the plan area in its entirety last occurred in May 1994, with the approval of the SMA for Blandenburg-New Carrollton and Vicinity (Planning Area 69) by Council Resolution CR-53-1994.

Conditional Zoning

Safeguards, requirements, and conditions beyond the normal provisions of the Zoning Ordinance that can be attached to individual zoning map amendments via “conditional zoning” cannot be utilized in SMAs. In the piecemeal rezoning process, conditions are used to (1) protect surrounding properties from potential adverse effects that might accrue from a specific zoning map amendment; and/or (2) to enhance coordinated, harmonious and systematic development of the Regional District. When approved by the District Council and accepted by the zoning applicant, “conditions” become part of the County Zoning Map requirements applicable to a specific property and are as binding as any provision of the County Zoning Ordinance (see Conditional Zoning Procedures, Section 27-157(b)).

In theory, zoning actions taken as part of the comprehensive zoning (SMA) process should be compatible with other land uses without the use of conditions. However, it is not the intent of an SMA to repeal the additional requirements determined via conditional zoning cases that have been approved prior to the initiation of a sectional map amendment—or since initiation but before approval. As such, when special conditions to development of specific properties have been publicly agreed upon and have become part of the existing zoning map applicable to the site, it is appropriate to bring those same conditions forward in the SMA. This is accomplished by approving zoning with conditions and showing the zoning application number on the newly adopted zoning map. This would take place only when it is found that the existing zoning is compatible with the intended zoning pattern or when ordinance limitations preclude a rezoning. Similarly, findings contained in previously approved SMAs shall be brought forward in the SMA where the previous zoning category has been maintained.

Comprehensive Design Zones

Comprehensive design zones may be included in a sectional map amendment. Normally, the flexible nature of these zones requires a basic plan of development to be submitted through the zoning application process (zoning map amendment) in order to evaluate the comprehensive design proposal. It is only through approval of a basic plan, which identifies land use types, quantities, and relationships, that a comprehensive design zone can be recognized. Therefore an application must be filed, including a basic plan, and the Planning Board must have considered and made a recommendation on the zoning application in order for the comprehensive design zone to be included in the SMA. During the comprehensive rezoning, prior to the submission of such proposals, property must be classified in a conventional zone that provides an appropriate “base density” for development. In theory, the base density zone allows for an acceptable level of alternative development should the owner choose not to pursue full development potential indicated in the master plan.

Comprehensive Rezoning Changes

Some uses in specific portions of the Central Annapolis Road Area are in line with the future vision for the plan area. The existing pattern of residential development is retained to maintain the unique character of established neighborhoods.

Other parcels of land must be rezoned to bring the zoning into conformance with the sector plan. Figure 8.2 illustrates the existing land use plan, and Figure 8.3 illustrates the proposed land use plan. This evolution of the corridor as illustrated in these plans reflects the vision for the overlay area as established through the Central Annapolis Road Corridor Plan—the guiding policy document for the zoning changes included in this sectional map amendment. The comprehensive rezoning process (via the SMA) provides the most appropriate mechanism for the public sector to achieve consistency. As such, the SMA is approved as an amendment to the official map(s) concurrently with approval of the sector plan.

The Central Annapolis Road SMA makes three zoning changes based on the land use and development policies of the sector plan. Application of mixed-use zoning techniques are required to implement the long-range land use recommendations of the Central Annapolis Road Corridor Sector Plan for integrated centers of community activity along the corridor. Two mixed-use zones are recommended: the M-U-I (Mixed-Use Infill) Zone and the M-X-T (Mixed-Use Transportation) Zone.

The M-U-I (Mixed-Use Infill) Zone: The primary purpose of this zone is to encourage residential, commercial, mixed-residential, and commercial development in established communities. The uses permitted in an M-U-I Zone are the same as those permitted by right or by exception in the Commercial Shopping Center (C-S-C) Zone. However, for use category (3) Miscellaneous and

use category (6) Residential/Lodging, the uses allowed are those permitted in the medium-density R-18 Zone.

The M-X-T (Mixed-Use Transportation) Zone: The primary purpose of this zone is to foster a pedestrian-scale, community-oriented place adjacent to the proposed Purple Line stop at Veterans Parkway and Annapolis Road. The zone mandates at least two of the following three use categories: (1) retail business, (2) office/research/industrial, and (3) dwellings, hotel/motel. The zone also encourages a 24-hour functional environment and builds on existing public infrastructure investment.

The SMA also recommends that a Development District Overlay Zone (DDOZ) be applied to the entire SMA to achieve the plan's goal of creating a multinode transportation corridor with a community focus. The DDOZ is a mapped zone that is superimposed by an SMA over the other zones in a designated development area and may modify development requirements and/or standards within the underlying zone.

Under the DDOZ for Central Annapolis Road, new development plans or redevelopment plans are reviewed for their compliance with development standards approved in the sector plan and the SMA. If proposed development reflects the development standards recommended for each of the character areas, the review can take place in the permit review process; however, there will be no walk-through permits in the framework character areas (see figure 8.9). For developments of four or more acres and for view terminus sites, detailed site plans are reviewed and approved by the Planning Board.

Figure 8.4 illustrates the proposed zoning, and Table 8.1 summarizes the cumulative zoning inventory.

The plan vision presented in Chapter 6 calls for the long-range development of mixed-use residential (multifamily housing over ground-floor retail) along the south side of Annapolis Road opposite Capital Plaza some time after the year 2025. Rezoning of the commercially zoned properties in this area will be required in order to permit this development to occur. However, because of the extended development time frame involved, the plan does not recommend rezoning this area at this time.

Figure 8.2 Existing Land Use

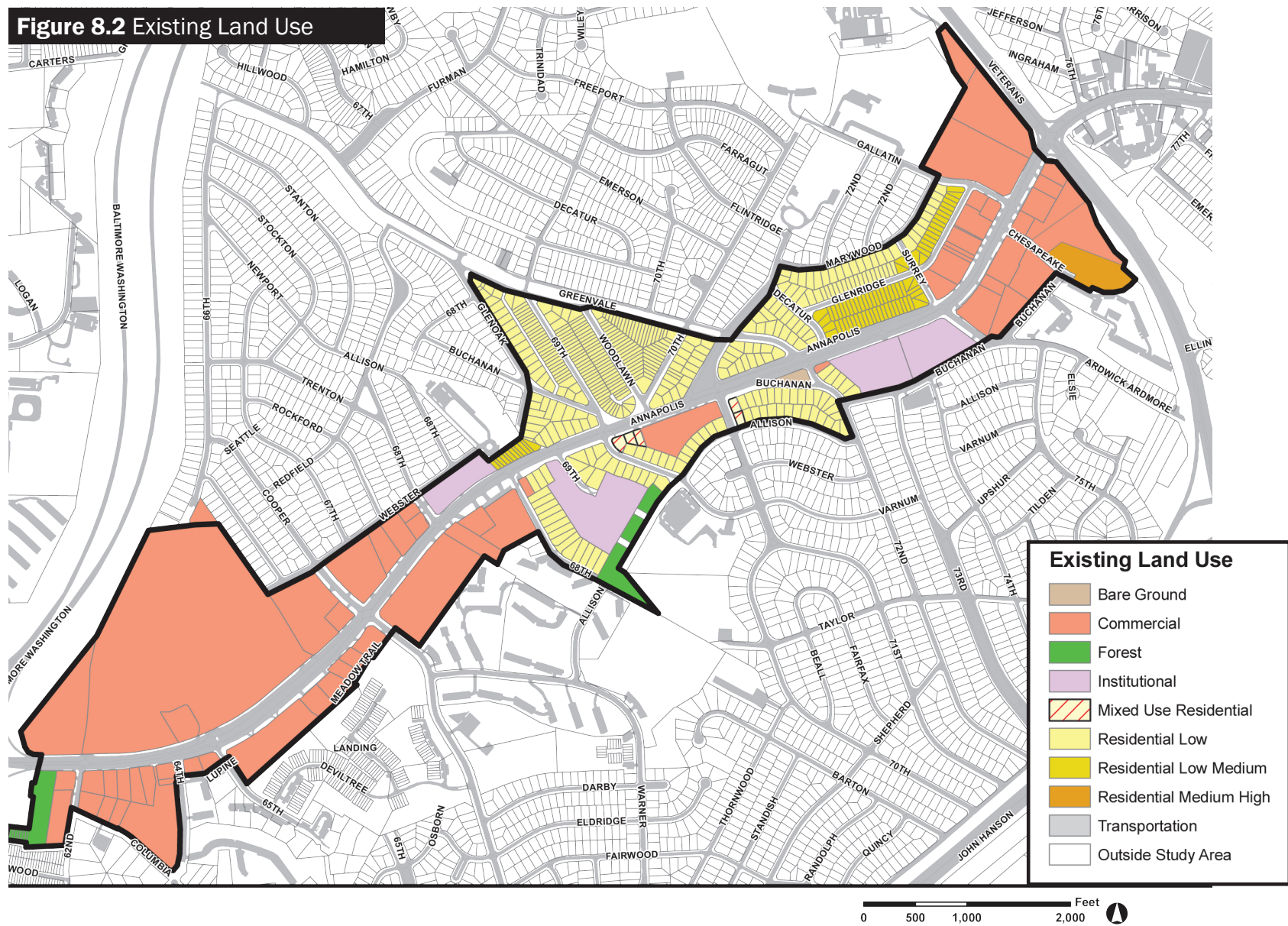


Figure 8.3 Proposed Land Use

Future Land Use

- Bare Ground
- Commercial
- Forest
- Institutional
- Mixed Use Commercial
- Mixed Use Residential
- Residential Low Density
- Residential Medium Density
- Transportation
- Future Transit Line
- Outside Study Area

0 500 1,000 2,000 Feet

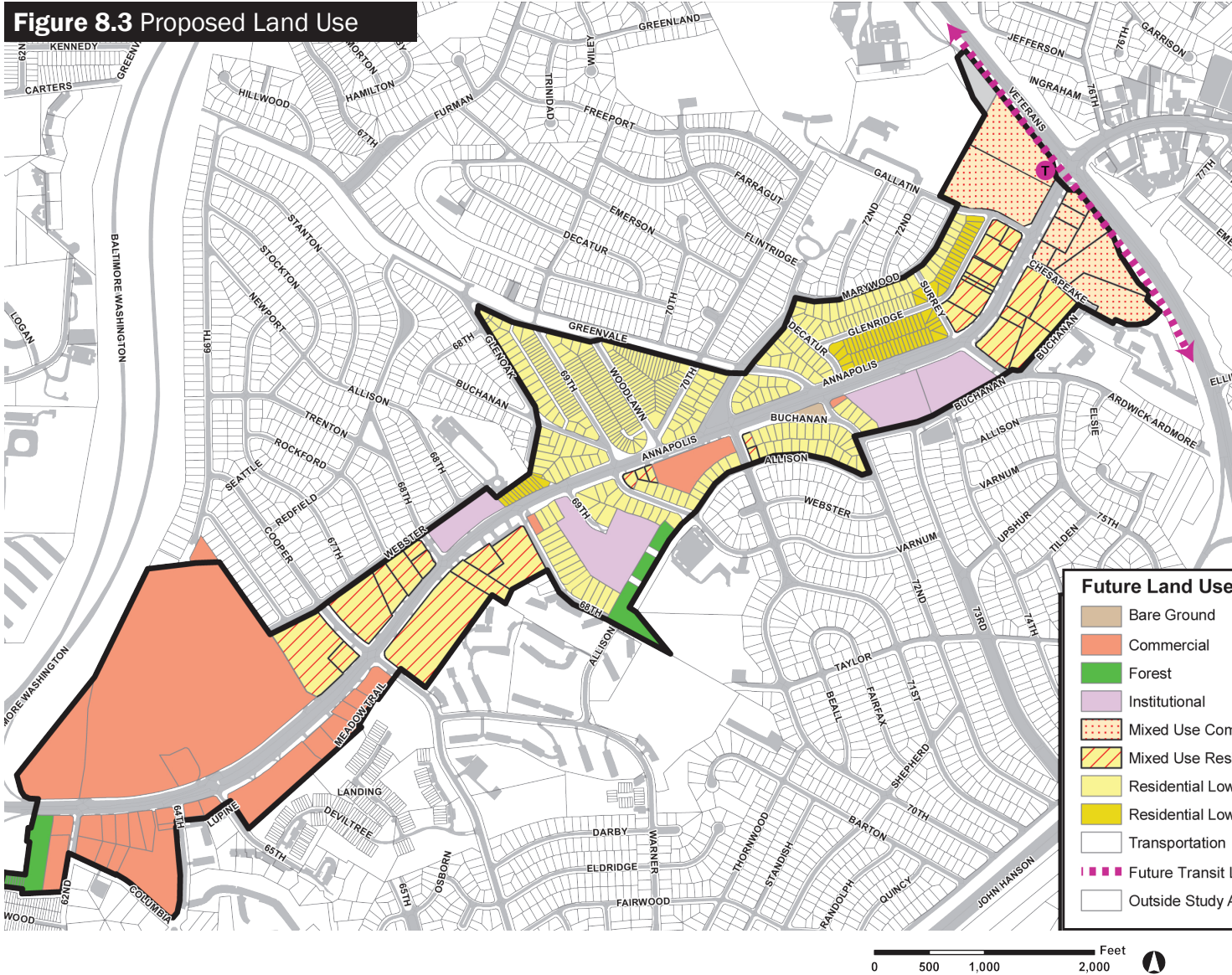


Figure 8.4 Proposed Zoning

Legend:

- C-A (Ancillary Commercial)
- C-M (Commercial Miscellaneous)
- C-S-C (Commercial Shopping Center)
- C-O (Commercial Office)
- M-U-I (Mixed-use infill)
- M-X-T (Mixed-use Transportation-oriented)
- R-20 (One-fam. Triple-attached residential)
- R-35 (One-fam Semidetached/Two-fam Detached Residential)
- R-55 (One-family Detached Residential)

Table 8.1 Cumulative Zoning Inventory

ZONING DISTRICT	EXISTING		PROPOSED	
	ACREAGE	% OF LAND AREA	ACREAGE	% OF LAND AREA
C-A Ancillary Commercial	0.13	0.06%	0.13	0.06%
C-M Commercial Miscellaneous	5.56	2.70%	4.79	2.33%
C-O Commercial Office	9.94	4.83%	.95	.46%
C-S-C Commercial Shopping Center	114.42	55.76%	75.38	36.63%
R-20 One-Family Triple Attached Residential	0.89	0.43%	0.89	0.43%
R-35 One-Family Semidetached, Two-Family Detached Residential	7.35	3.57%	7.35	3.57%

ZONING DISTRICT	EXISTING		PROPOSED	
	ACREAGE	% LAND AREA	ACREAGE	% LAND AREA
C-A Ancillary Commercial	0.13	0.06%	0.13	0.06%
C-M Commercial Miscellaneous	5.56	2.70%	4.79	2.33%
C-O Commercial Office	9.94	4.83%	.95	.46%
C-S-C Commercial Shopping Center	114.42	55.76%	75.38	36.63%
R-20 One-Family Triple Attached Residential	0.89	0.43%	0.89	0.43%
R-35 One-Family Semidetached, Two-Family Detached, Residential	7.35	3.57%	7.35	3.57%
R-55 One-Family Detached Residential	65.38	31.78%	65.44	31.80%
R-T Townhouse	1.78	0.87%	0	0
M-U-I Mixed Use Infill	0	0	29.04	14.11%
M-X-T	0	0	21.79	10.59%
TOTAL	205.75	100.00	205.76	100%

Table 8.2 Zoning Change #1

CHANGE NUMBER	ZONING CHANGE	AREA OF CHANGE	APPROVED SMA/ZAPS/SE		200' MAP	SCALE	INDEX
			NUMBER	DATE			
1	Superimpose DDOZ on C-A, C-S-C, C-O, C-M, R-20, R-35, R-55, R-T	252.7 Ac.	SMA	1/28/93	205NE05 205NE06 206NE06		

Use and Location: All properties within the boundaries of the proposed Central Annapolis Road Sectional Map Amendment.

Discussion: The proposed Development District Overlay Zone (DDOZ) will impose urban design standards and guidelines developed to implement the plan vision for a corridor of walkable, connected centers that serve regional destinations at Capital Plaza and Glenridge while providing services to the surrounding community.

Figure 8.5 Zoning Change #1

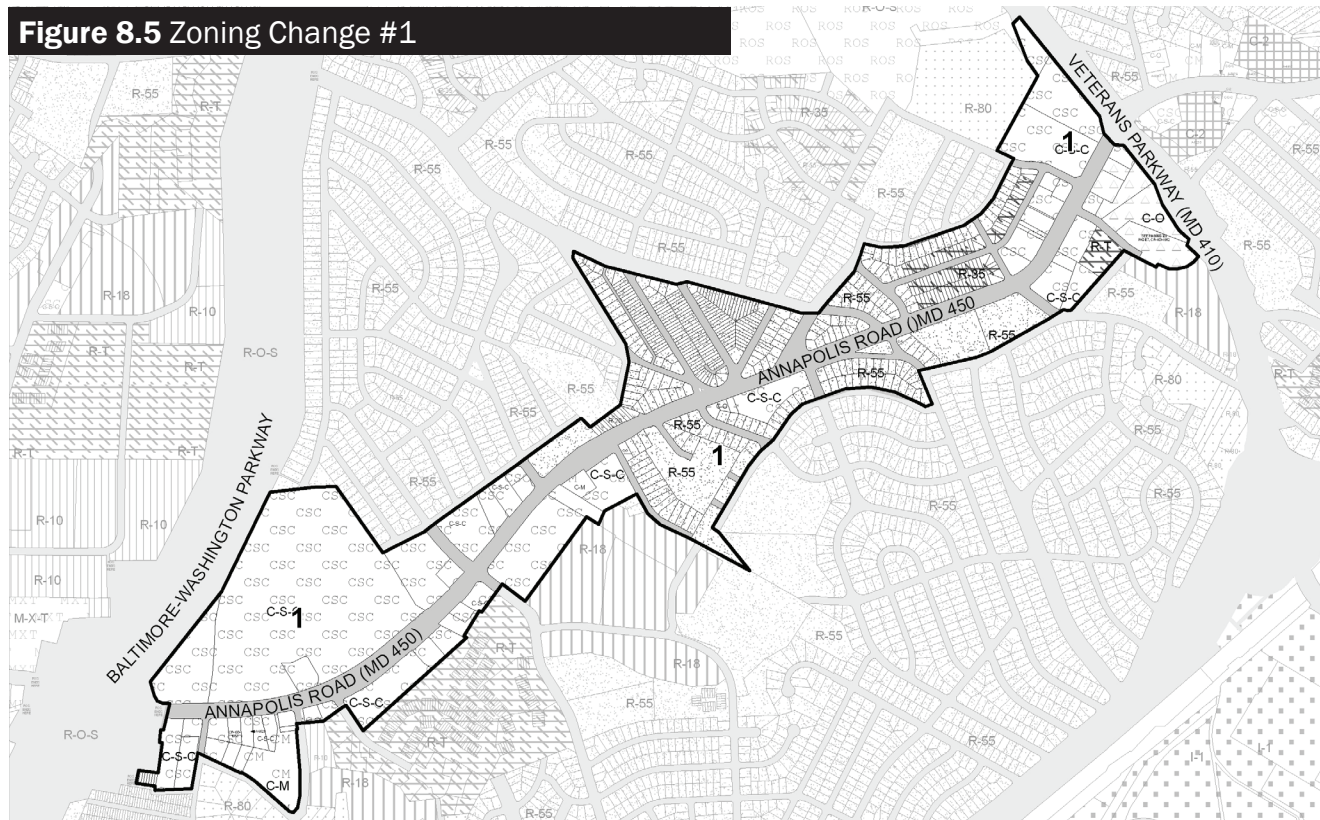


Table 8.3 Zoning Change #2

CHANGE NUMBER	ZONING CHANGE	AREA OF CHANGE	APPROVED SMA/ZAPS/SE		200' MAP	SCALE	INDEX
			NUMBER	DATE			
2	C-O to M-X-T C-S-C to M-X-T Total	9.00 Ac. <u>12.37 Ac.</u> 21.37 Ac.	SMA	1/28/93	206NE06		

Use and Location: Annapolis Road addresses—Retail shopping center at 7522-58; dental and medical clinic at 7503; office building at 7515; vacant building at 7519
Chesapeake Road addresses—condominium offices at 7050, 7100, 7150
(Tax Map 051, Grid E1, GLENRIDGE SHOPPING CENTER PARCELS A, B, B-1; WEST LANHAM SHOPPING CENTER PARCELS A, B, C; CHESAPEAKE OFFICE PARK CONDO all units; PARCELS 21, 143)

Discussion: Rezoning of these properties from C-O and C-S-C to M-X-T will allow for redevelopment of these properties with mixed-use residential and retail/office uses consistent with the plan vision of transit-oriented development in the the Glenridge Transit Village character area.

Table 8.4 Zoning Change #3

CHANGE NUMBER	ZONING CHANGE	AREA OF CHANGE	APPROVED SMA/ZAPS/SE		200' MAP	SCALE	INDEX
			NUMBER	DATE			
3	R-T to M-U-I C-S-C to M-U-I Total	1.78 Ac. <u>9.92 Ac.</u> 11.70 Ac.	SMA SE-982 SE-41 SE-4251 SE-605 SE-1077	1/28/93	206NE06		

Use and Location: Annapolis Road addresses—nursery sales at 7400; pharmacy at 7401; gas station at 7460; laundromat at 7456; retail and service commercial at 7423-7467; fast-food restaurant at 7500
Gallatin Street address—funeral home at 7311
Buchanan Street addresses—Landover Hills Post Office at 7400; church at 7550
(Tax Map 051, Grid E1, GLENRIDGE SHOPPING CENTER PARCELS A, B, C; WEST LANHAM HILLS-RESUB OF BLK 114, Lots 1, 2; WEST LANHAM SHOPPING CENTER PARCEL D; Tax Map 051, Grid D1, GLENRIDGE, Block J, PARCELS A-H; PARCEL 155)

Discussion: Rezoning of these properties from R-T and C-S-C to M-U-I will allow for redevelopment of these properties with mixed-use residential and retail/office uses consistent with the plan vision of a mixed-use buffer between the Glenridge Transit Village character area and the existing single-family detached neighborhoods to the west.

Figure 8.6 Zoning Change #2 & 3



Table 8.5 Zoning Change #4

CHANGE NUMBER	ZONING CHANGE	AREA OF CHANGE	APPROVED SMA/ZAPS/SE		200' MAP	SCALE	INDEX
			NUMBER	DATE			
4	C-S-C to M-U-I	34.31 Ac.	SMA	1/28/93	205NE05		
	C-M to M-U-I	5.55 Ac.	SE-489				
	Total	39.76 Ac.	SE-1357				

Use and Location: Annapolis Road addresses—gas station at 6181, 6321, 6579, 6710; motel at 6205 3910; bank at 3911; restaurant at 6221 and 6401; check cashing at 6421; fast food restaurant at 6505, 6747; retail commercial at 6507-6541, 6551, 6611-6737; auto related commercial at 6545, 6815, 6825; health clinic at 6559; carpet sales at 6569; child care at 6801; church at 6706; laundromat at 6704

62nd Avenue address—motel at 3910

64th Avenue address—mini-storage at 3800

68th Avenue address—check cashing at 4606

Coopers Lane address—bowling alley 4601

(Tax Map 051, Grid A3, OAKLYN, Block F, lots 1-7, 16-18, OAKLYN-BROWNS ADDN, OAKLYN-RESUB PARCEL A, Parcel 50, 51; Tax Map 051, Grid A4, COLUMBIA HIGHLANDS, Block 7, Lots 1-8; Tax Map 051, Grid B3, OAKLYN, Block G, lots 1, 2; CAPITAL CORNER PARCEL A; MCGUIRE- PT OF THE R C TRACT PARCEL D, E; LANDOVER ESTATES, Block 20, PARCEL A and Block 19, PARCEL D; GRAYLING, BLOCK E, LOT 5-7; Tax Map 051, Grid B2, CARROLL CORP PROPERTY PARCEL A; DEFENSE HEIGHTS, Block A lot 1, Block B, PARCEL A, B, C-4, p/o D, p/o OUTLOT A; Parcel 53, 99)

Discussion: The proposed rezoning of the properties will facilitate redevelopment of the blocks by permitting a mix of uses and densities in accordance with the plan vision of a mixed-use buffer between the existing single-family detached residences to the east and the Capital Plaza retail town center to the west.

Development District Overlay Zone

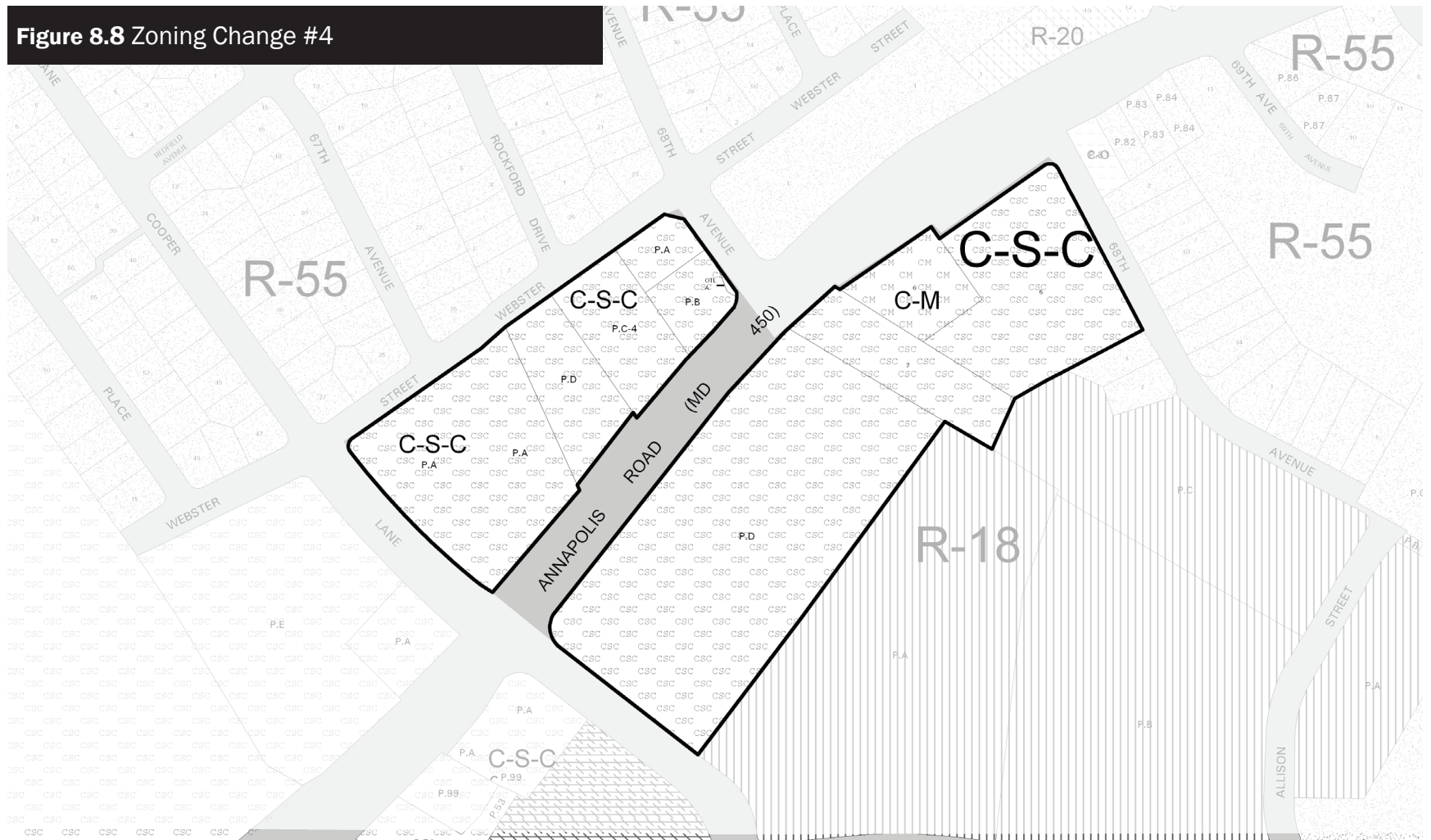
Introduction to the Development District Overlay Zone

The Development District Overlay Zone (DDOZ) is superimposed over the Central Annapolis Road Sector Plan area to ensure that development of land meets the goals of the plan. The sector plan offers a vision and sets goals for the future development of the Central Annapolis Road Corridor. Specific recommendations address land use, urban design, transportation, and public facilities, with an implementation program to guide revitalization and redevelopment.

The DDOZ standards follow and implement the recommendations of the Central Annapolis Road Plan, including:

- *Glenridge Transit Village*
 - > Foster long-term redevelopment with a safe and inviting pedestrian experience and potential frontage road access.
 - > Preserve existing neighborhoods and create appropriate transitions to neighboring development.
- *Existing Residential Neighborhood*
 - > Retain existing character of single-family homes.
 - > Create significant buffers between homes and arterial routes.

Figure 8.8 Zoning Change #4



- *Mixed-use Transition Area*
 - > Create a transition between single-family housing and retail shopping centers.
 - > Foster mixed-use redevelopment and infill.
- *Retail Town Center*
 - > Foster large parcel redevelopment into neighborhood shopping center.
 - > Enhance pedestrian and multiple-stop vehicular trips.
 - > Provide safer bicycle and pedestrian links between public rights-of-way and retail storefronts.
 - > Support an appropriate mix of uses within large sites and the district as a whole.

Consistency with the General Plan

The 2002 *Prince George County General Plan* identifies centers in the county as areas for concentrating medium- to high-intensity, mixed-use, and pedestrian-oriented development. Consistent with the Central Annapolis Road Sector Plan, the General Plan is amended to designate the intersection of Veterans Parkway and Annapolis Road as a center simultaneous to the adoption of this sectional map amendment.

The 2009 *Countywide Master Plan of Transportation* (MPOT) identifies proposed right-of-way dimensions for various classifications of roadways. The DDOZ reflects this plan by establishing required building setbacks that accommodate proposed right-of-way, as well as other future transportation improvements identified in the Central Annapolis Road Sector Plan. For the purposes of this DDOZ, front yard setbacks are to be measured from the Annapolis Road corridor street centerline. For the purposes of this DDOZ, the location of the

Annapolis Road corridor centerline is on file with the Maryland State Highway Administration (SHA).

Applicability and Administration

The regulations and requirements of the Prince George's County Zoning Ordinance apply to the DDOZ unless the Central Annapolis Road development standards specify otherwise. Property owners and citizens consulting the standards must also review the goals and objectives of the sector plan, the Zoning Ordinance, the subdivision regulations (Subtitle 24), and the *Prince George's County Landscape Manual* to have full understanding of the regulations for property within the district.

Development in the Central Annapolis Corridor DDOZ is subject to the development district standards as detailed below. All new development and redevelopment of existing structures within the DDOZ shall comply with the intent and the development district standards and the Central Annapolis Road Corridor Plan. Development must show compliance during the detailed site plan process.

Under the Zoning Ordinance, and for the purposes of the Development District Overlay Zone, development is any activity that materially affects the condition or use of land or a structure. Redevelopment, rehabilitation, and renovation of existing structures are all forms of development. A change from a lower-intensity impact use to a higher-intensity impact use, as indicated in the *Prince George's County Landscape Manual*, is also a form of development.

Whenever there appears to be a conflict between the Central Annapolis Corridor DDOZ and the Prince George's County Zoning Ordinance (as applied to a particular development), the DDOZ shall prevail. For development standards not covered by the DDOZ, the other applicable sections of the

Zoning Ordinance and the Landscape Manual shall serve as the requirement. All development shall comply with all relevant federal, state, county, and local regulations and ordinances.

Exemptions from the Development District Standards

The following are exemptions from the development district standards:

- 1. Legally existing development.** Until a site plan is submitted, all buildings, structures, and uses which were lawful or could be certified as a legal nonconforming use on the date of SMA approval are exempt from the development district standards and from site plan review and are not nonconforming.

Until a site plan is submitted, active shopping centers with freestanding commercial uses on perimeter pod sites are also exempt from the DDOZ standards and from site plan review and are not nonconforming. However, the issuance of a Building Permit or a Use and Occupancy Permit for a change in ownership for any property with frontage along a public street shall require restoration or installation of landscape strips, buffering, and screening in accordance with Sections 4.2 and 4.4 of the Landscape Manual, as modified by the streetscape standards of this DDOZ, or as determined under an Alternative Compliance procedure per Section 1.3 of the Landscape Manual. The plan recommends that shopping center owners consider developing plans for the phased redevelopment of their properties to new mixed-use urban places.

- 2. Legally existing parking and loading.** Until a site plan is submitted, all legally existing parking and loading spaces in the development district that were lawful and not nonconforming on the date of the SMA approval are exempt from the development district standards and site plan review, need not be reduced, and are not nonconforming.
- 3. Single-family residential dwellings.** Additions to single-family residential dwellings are exempt from the

development district standards and site plan review, if the residential use continues.

- 4. Multifamily development.** An addition to a multifamily residential structure that was lawful and not nonconforming on the date of the SMA approval is exempt from the development district standards and site plan review if the addition (and accumulated sum of all additions since approval of the SMA) does not increase the gross floor area (GFA) by more than 15 percent or 5,000 square feet, whichever is less.
- 5. Nonresidential development.** An addition to a nonresidential structure that was lawful and not nonconforming on the date of the SMA is exempt from the development district standards and site plan review, if the addition (and the accumulated sum of all additions since the approval of the SMA) does not increase the GFA by more than 15 percent or 5,000 square feet, whichever is less.
- 6. Parking facilities.** Resurfacing, restriping, or adding landscaping to parking facilities not required by the standards are exempt from the development district standards and site plan review, if the facilities were lawful and nonconforming on the date of SMA approval and remain in conformance with all previous applicable regulations.
- 7. Nonconforming buildings, structures, and uses.** Restoration or reconstruction of a nonconforming building or structure, or a certified non-conforming use, is exempt from the development district standards and from site plan review if it meets the requirements of Section 27-243(a)(1) of the Zoning Ordinance.

Except for improvements listed in 8. General below, a property may not expand a certified nonconforming use or a use or a structure that was lawful on the date of the SMA approval but does not conform to the development district standards, unless a detailed site plan is approved with findings that the expansion is compatible with adjacent uses and meets the goals of the sector plan.

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- 8. General.** The following are exempt from the development district standards and site plan review if the existing or proposed use is permitted:
- Permits for alternation or rehabilitation, with no increase of the existing gross floor area
 - Canopies
 - Fences of six feet in height or less for rear and side yards and made of wood or masonry (not concrete block) are exempt
 - Decks
 - Ordinary maintenance
 - Changes in occupancy
 - Changes in ownership
- 9. Signs.** Signs in a development requiring a detailed site plan will be reviewed in the site plan process. Signs for development not otherwise requiring a detailed site plan will be reviewed in the permit review process for compliance with the development district standards.

Public Improvements

Within the Central Annapolis Road DDOZ, the developer/property owner (including the developer and the applicant's heirs, successors and assignees) is required to construct (or contribute funds toward the construction of) all new development-related streetscape improvements outside of the public right-of-way maintained by the Maryland State Highway Administration (SHA), the county's Department of Public Works and Transportation (DPW&T), or the Town of Landover Hills. Developer/property owners shall also be required to maintain streetscape improvements outside of the public right-of-way. New development projects with a gross floor area (GFA) of less than 10,000 square feet shall be exempt from the public improvement requirements of the Central Annapolis Road DDOZ.

Site Plan Submittal Requirements

The detailed site plan submittal requirements for the Central Annapolis Road Development District Overlay Zone are the same as those required by Part 3, Division 9, of the Zoning Ordinance.

Applicants are encouraged to meet with the Planning Department staff while developing the project concept (well in advance of final plans) to review submittal requirements for a detailed site plan per Part 3, Division 9, of the Zoning Ordinance and applicable development district standards, to obtain preliminary evaluation of foreseeable conformance issues and to identify required documentation.

Other pertinent information required for detailed site plan submittals as per Section 27-282(e)(20) shall include:

- Architectural elevations in color
- Street and streetscape sections
- Setbacks
- Parking schedule and plan
- Supporting documentation where requested in the development district standards

Applicants shall provide a list of all applicable standards from this document that have been used in the design, as well as a list of standards that have not been fulfilled and explanations as to why they have not been fulfilled.

Uses

The Central Annapolis Road Development District includes properties classified in the C-A, C-M, C-O, C-S-C, R-20, R-35, R-55, R-T, M-U-I and M-X-T. The uses allowed on these properties shall be the same as those allowed in the underlying zone in which the property is classified, except as modified by these development district standards.

Modification of the Development District Standards

This is permitted through the process described in Section 27-548.25(c) of the Zoning Ordinance. “If the applicant so requests, the Planning Board may apply development standards which differ from the Development District Standards most recently approved or amended by the District Council, unless the Sectional Map Amendment text specifically provides otherwise. The Planning Board shall find that the alternative Development District Standards will benefit the development and the development district and will not substantially impair implementation of the Master Plan, Master Plan Amendment, or Sector Plan.” Three types of amendments are required to be heard by the District Council: changes in the boundary of the DDOZ; changes in the underlying uses and to the list of permitted uses; and changes in any other specifically designated standards.

These amendments may take the form of a detailed site plan. Equivalent or better practices and products than those specified are always encouraged and may be submitted for approval.

As set forth in Section 27-108.01(a)(15) of the Zoning Ordinance, “The words ‘including’ and ‘such as’ do not limit a term to the specified examples, but are intended to extend its meaning to all other instances or circumstances of like kind or character.” As set forth in Section 27-108.01(a) (19) of the Zoning Ordinance, “the words ‘shall,’ ‘must,’ ‘may only,’ or ‘may not’ are always mandatory and not discretionary. The word ‘may’ is permissive.”

Unless otherwise stated, these development district standards replace the standards and regulations required by the Zoning Ordinance of Prince George’s County. Except as modified by the building envelope standards, development is subject to the minimum lot area requirements of the Zoning Ordinance for the underlying zoning.

Development proposals evaluated under these regulations should be measured against the general intent and desired character for the Central Annapolis Road Development District as established in the sector plan.

Understanding the Development District Overlay Zone (DDOZ)

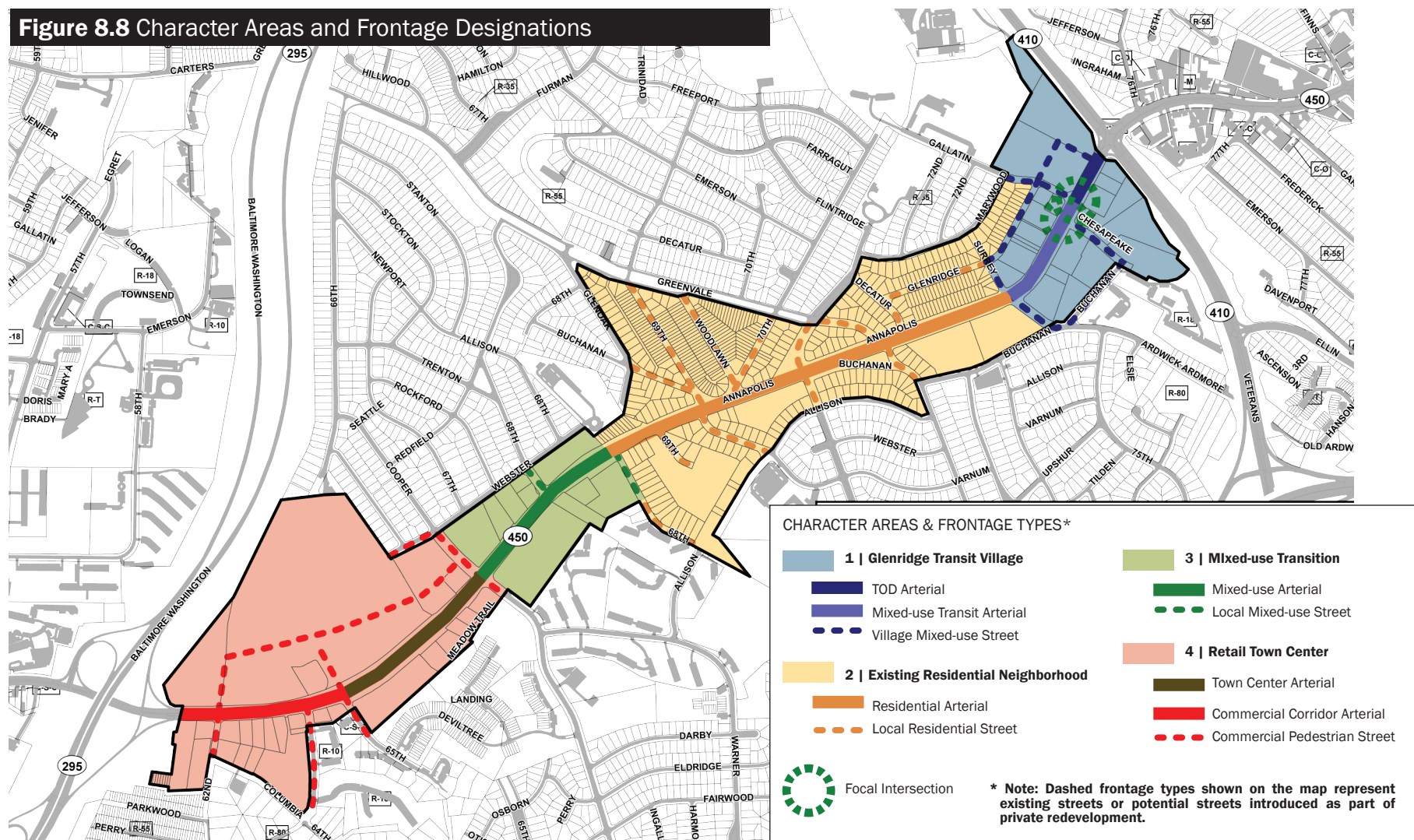
The development district standards are specifically intended to address new development and redevelopment proposals in the development district. The standards establish a consistent design framework to ensure quality in future development.

The DDOZ provides standards for the development of each property or lot and illustrates how each relates to the adjacent properties and street. All public streets (i.e., streets and alleys in the public right-of-way) and private roadways (i.e., frontage access roads on private parcels, internal shopping center streets, etc.) shall be designated as one of the frontage types established below.

Each parcel is identified by its development character and street frontage. Regulations are subsequently linked to character type or frontage type where applicable. More specific regulations may apply at focal intersections that serve as centers of activity or major neighborhood nodes.

All development parcels shall lie within one of the character areas hereby established below, and front on a designated frontage type as described within each character area.

Figure 8.8 Character Areas and Frontage Designations



Character Areas

Character areas provide the framework for development regulations that foster specific development forms that reflect the varying goals for each area. Within each character area, a series of frontage types are identified. These frontage types are broken down by the various types of roads within each character areas and allow development to respond to surrounding neighborhoods differently than to the Annapolis Road corridor.

1. Glenridge Transit Village

- a. The purpose of the Glenridge Transit Village Area is to promote a compact, mixed-use, and pedestrian-friendly center at the intersection of Veterans Parkway and Annapolis Road. The Glenridge Transit Village Area will include medium-density mixed-use development resulting in active streets, unique public spaces, and high levels of pedestrian and transit access. Development controls for the area focus on the creation of a high-quality walkable and transit-accessible built environment.
- b. Associated frontage types
 - i. *TOD Arterial*—Annapolis Road between Veterans Parkway and Gallatin Street
 - ii. *Mixed-use Transit Arterial*—Annapolis Road and potential future frontage access roads between Surrey Lane and Gallatin
 - iii. *Village Mixed-use Street*—Glenridge between Surrey Lane and Gallatin Road

2. Existing Residential Neighborhood

- a. The purpose of the Existing Residential Neighborhood Area is to conserve the quality of life and existing pattern of development in the existing single-family residential neighborhoods. The Existing Residential Neighborhood

Area will include primarily single-family or duplex housing. Development controls in this area aim to preserve the character of single-family blocks by incorporating building massing consistent with the existing structures and landscaping that creates an attractive link between adjacent mixed-use areas.

- b. Associated frontage types
 - i. *Residential Arterial*—The north side of Annapolis Road between 68th Avenue and Surrey Lane, and the south side of Annapolis Road between 68th Avenue and Ardwick-Ardmore Road
 - ii. *Local Residential Street*—All side streets

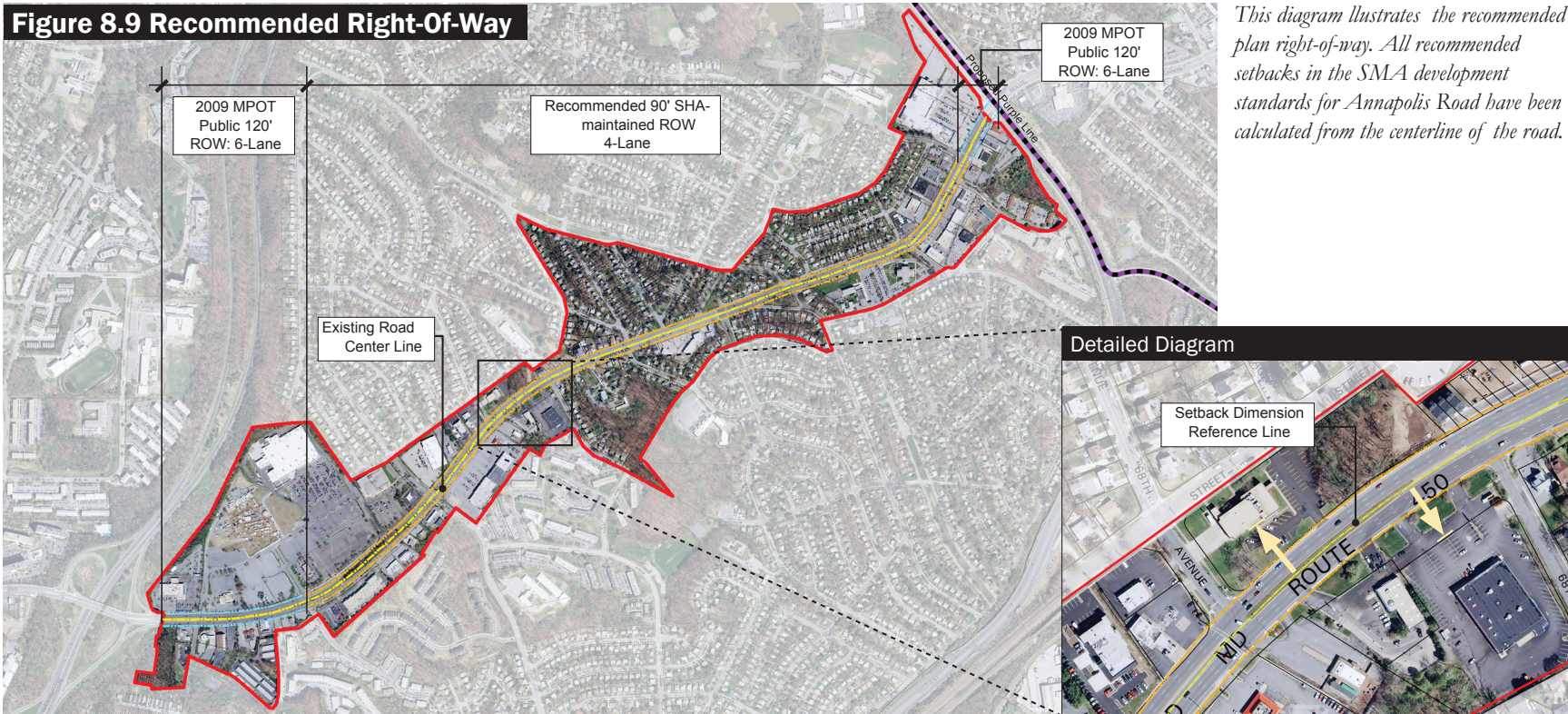
3. Mixed-use Transition

- a. The purpose of the Mixed-use Transition Area is to promote medium-density mixed-use with a residential character along segments of Annapolis Road currently occupied by underutilized strip commercial development. The Mixed-use Transition Area will include a mix of commercial, mixed-use, and multifamily development. Development controls for this area aim to create viable residential blocks and active commercial uses that are responsive to local needs and access.
- b. Associated frontage types
 - i. *Mixed-use Arterial*—The north side of Annapolis Road between 66th Avenue and 68th Avenue, and the south side of Annapolis Road between Cooper Lane and 68th Avenue
 - ii. *Local Mixed-use Street*—All side streets, including internal circulator roads within multiple-tenant shopping centers

4. Retail Town Center

- a. The purpose of this area is to promote the redevelopment of a regional shopping destination in a town center environment. The Retail Town Center area will include regional or subregional concentrations of commercial retail uses and tenants, and may be composed of large parcels with multiple buildings and tenants, or standard parcels with individual structures. Development controls for this area address building design, parking location and access, and landscaping, and they include regulations related to the development of an internal street network and the design of public streets.
- b. Associated frontage types
 - i. *Commercial Corridor Arterial*—The north side of Annapolis Road between the Baltimore-Washington Parkway and 66th Avenue, and the south side of Annapolis Road between the Baltimore-Washington Parkway and Cooper Lane
 - ii. *Commercial Pedestrian Street*—All side streets, including internal circulator roads within multiple-tenant shopping centers

Figure 8.9 Recommended Right-Of-Way



Development District Overlay Zone (DDOZ) Standards

This section identifies standards and guidelines for development in each of the character areas. All development must conform to the development standards and follow the guidelines to the greatest extent possible as determined through the required design review process.

Recommended Right-Of-Way

The existing Annapolis Road right-of-way contains a six-lane divided highway between Baltimore-Washington Parkway (MD 295) and Veterans Parkway (MD 410). The 2009 *Approved Countywide Master Plan of Transportation* (MPOT) specifies an arterial right-of-way of 120 feet for Annapolis Road (see figure 8.9).

The Central Annapolis Road Sector Plan amends the 2009 MPOT public right-of-way for Annapolis Road between 65th Avenue and Gallatin Street. The amended SHA-maintained public right-of-way for this segment of Annapolis Road is 90 feet. The amended right-of-way permits the reconstruction of Annapolis Road as a four-lane divided highway with left-turn lanes at signal-controlled intersections. The recommended reduction in travel lanes is based on:

- The fact that John Hanson Highway (US 50), which parallels Annapolis Road inside the Capital Beltway, is a limited-access freeway that was built to divert regional through traffic from Annapolis Road.

- The traffic modeling and analysis performed as part of this plan's preparation indicates that a four-lane road is sufficient for the volume of existing and future traffic along this segment of Annapolis Road based on the plan vision for future development.

Annapolis Road between the Baltimore-Washington Parkway and 65th Avenue and between Gallatin Street and Veterans Parkway will remain a six-lane road within a 120-foot public right-of-way

A key mechanism for implementing the transformation of Annapolis Road according to the plan vision will be the establishment of a public use easement for streetscape improvements outside of the public right-of-way maintained by SHA. Unlike neighboring Montgomery County, Prince George's County currently has no such mechanism in place. The plan recommends that enabling legislation be prepared and enacted to implement public use easements in selected Centers and Corridors where future development is slated to take place. The plan also recommends the establishment of a revolving infrastructure improvement fund, financed partially by developer contributions, to implement the long-term reconstruction of Annapolis Road on a block-by-block basis as future development occurs.

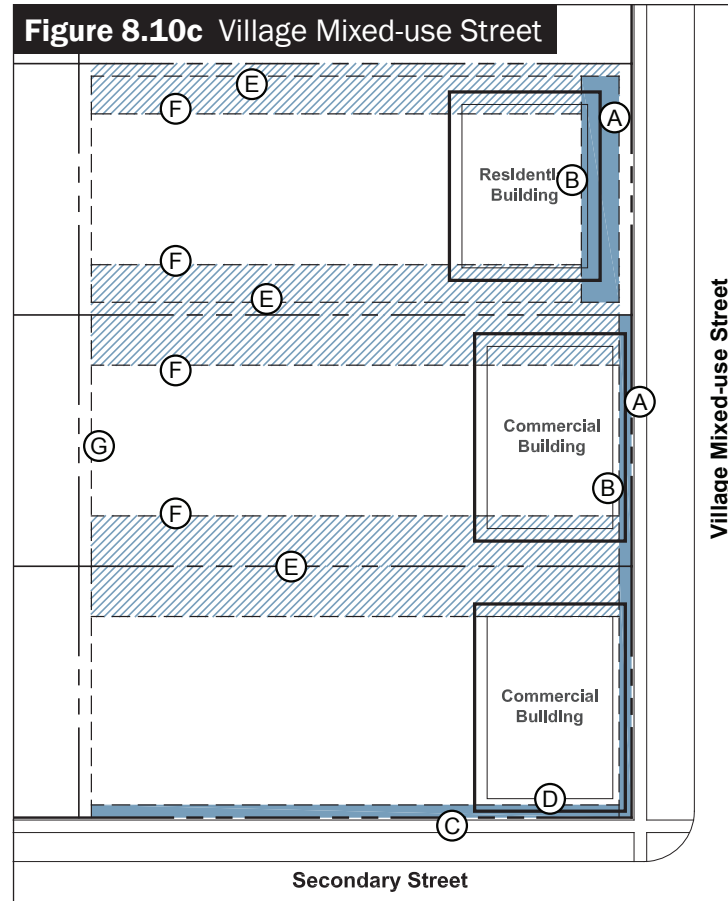
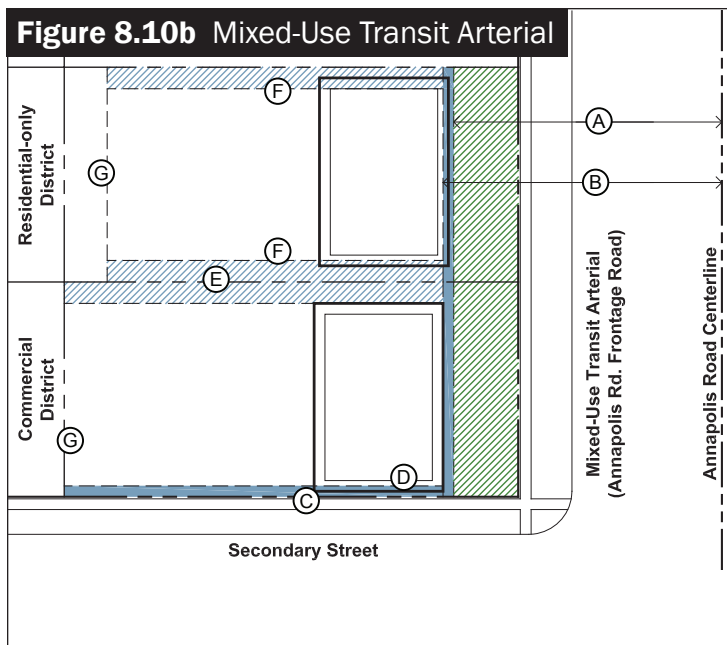
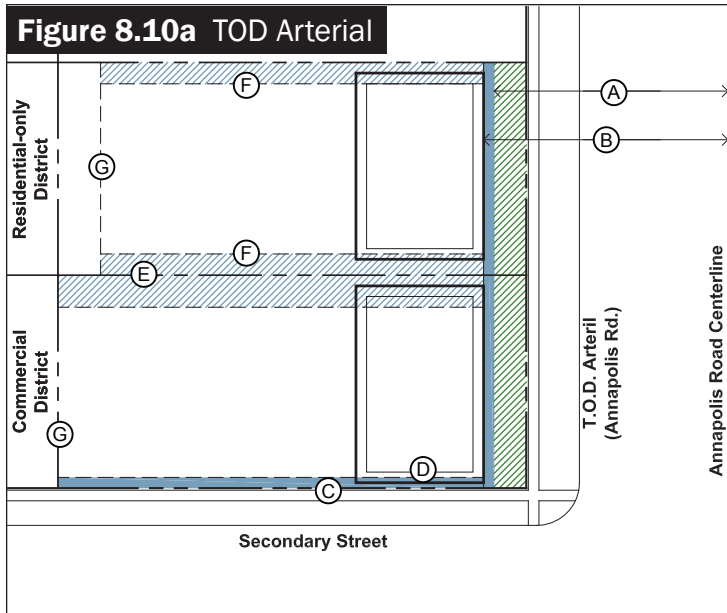
1. Glenridge Transit Village

- a. Table 8.6 summarizes bulk and yard requirements for the Glenridge Transit Village Area. Figures 8.10a through 8.10c illustrate the regulations as described in the table.

Table 8.6 Glenridge Transit Village Bulk Table

	PRIMARY FRONTAGE TYPE	TOD ARTERIAL	MIXED-USE TRANSIT ARTERIAL	VILLAGE MIXED-USE STREET
Front Building Placement Line				
(A)	Minimum	60'*	80'*	5' for residential-only buildings, otherwise 0'
(B)	Maximum	65'*	85'*	20' for residential-only buildings, otherwise 5'
Corner Side Yard				
(C)	Minimum	0'	0'	5' for residential-only buildings, otherwise 0'
(D)	Maximum	5'	5'	10' for residential-only buildings, otherwise 5'
Interior Side Yard				
(E)	Minimum	0'	0'	5'
(F)	Maximum	Aggregate of both interior side yard setbacks not to exceed 20% of lot width, excluding the width of an access drive to the primary street		
Rear Yard				
(G)	0', unless against a residential-only area, then 20'	0', unless against a residential-only area, then 20'		5'
Building Height				
	Maximum	70'		
Ground-Floor Height				
	Minimum	12'		

*Note: Placement Lines for the TOD Arterial and Mixed-use Transit Arterial frontage types are to be measured from the Annapolis Road centerline. All required yard setbacks are to be measured from the corresponding parcel line.



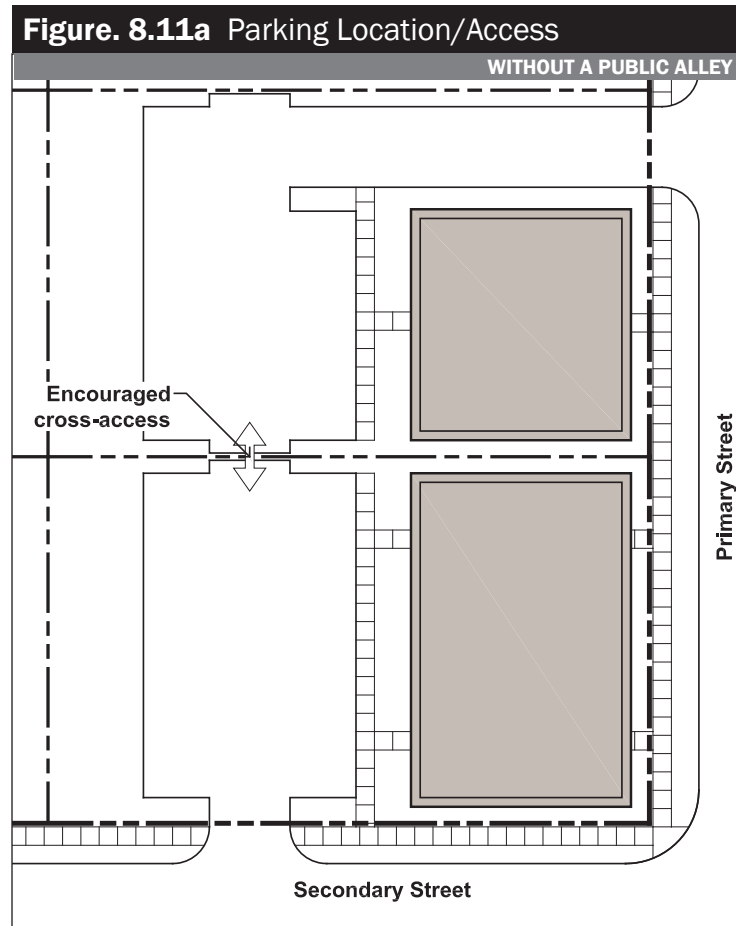
b. Parking and access management

Regulations in this area focus on creating and maintaining a strong pedestrian environment. Automotive access is accommodated, but it is anticipated that one vehicular trip may provide for several stops accessible by foot.

- (1) No parking shall be located in the front, corner side, or interior side yards.
- (2) Direct pedestrian access from the primary public sidewalk shall be provided to each tenant via sidewalks and a front façade entry, or a sidewalk to a shared lobby that provides direct internal access to ground-floor tenants.
- (3) Direct pedestrian access shall be provided from rear parking areas to tenant spaces or to a public lobby that provides access to tenant spaces.
- (4) No building or series of buildings shall be more than 250 feet in continuous frontage without providing public pedestrian passage between the rear parking area and the public sidewalk on the primary street. (The pass through may be an interior corridor provided it is generally accessible to the public.)
- (5) Curb cut access from the primary frontage street should be minimized wherever possible through shared curb cut access and cross-access between commercial tenants.
 - (a) For lots with less than 200 feet of frontage and no alley or rear public street, one curb cut is permitted from a public street. On interior lots, this may be the primary street. On corner lots, a curb cut is only permitted from the side street (see figure 8.11a).
 - (b) For lots with 200 feet or more of frontage, with or without an alley, one additional curb cut, above and beyond what is permitted otherwise, is permitted from the primary street (see figure 8.11b).
 - (c) For lots with access to a public alley or rear public street, no curb cut from the primary street is

permitted, unless the lot frontage equals or exceeds 200 feet. Then, one curb cut is permitted as per item (b) above (see figure 8.11c).

- (d) For all lots with access to a public alley or rear public street, access to parking should be provided first from the alley or rear public street, then from a side street, and finally from a primary street only if necessary.



(6) In the Glenridge Transit Village Area, the minimum is the half of the current minimum number of spaces requirement and the maximum should be the minimum of the current number as determined by Section 27-574(b) of the Zoning Ordinance as it relates to on-site parking capacity in the M-X-T area.

(7) Multiple uses—whether located on property or properties held by one owner or on properties held by multiple owners—are encouraged to establish shared parking agreements and/or construct cross-access parking areas as alternatives to providing dedicated on-site parking for each use.

Figure 8.11b Parking Location/ Access

WITH A PUBLIC ALLEY

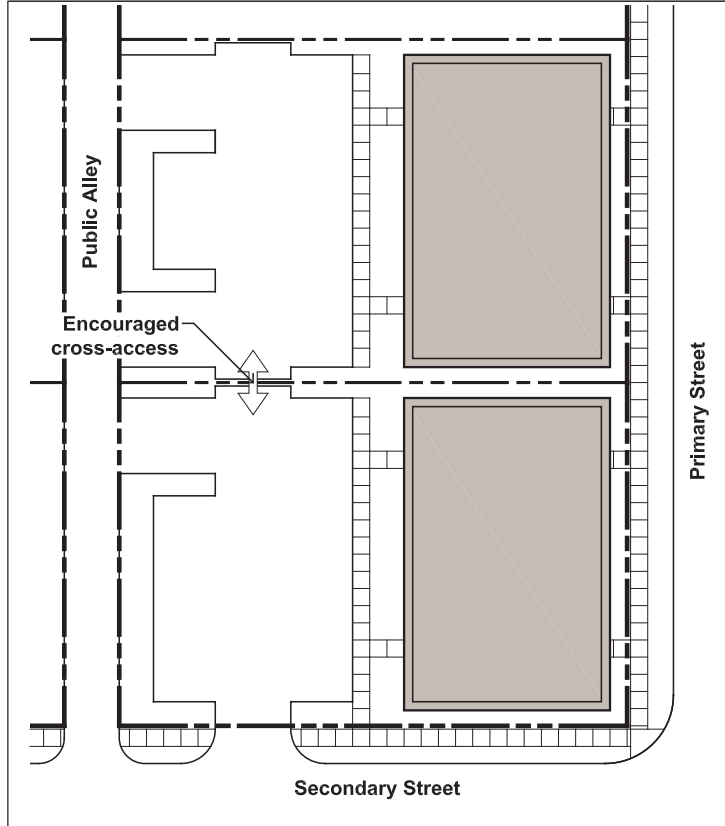
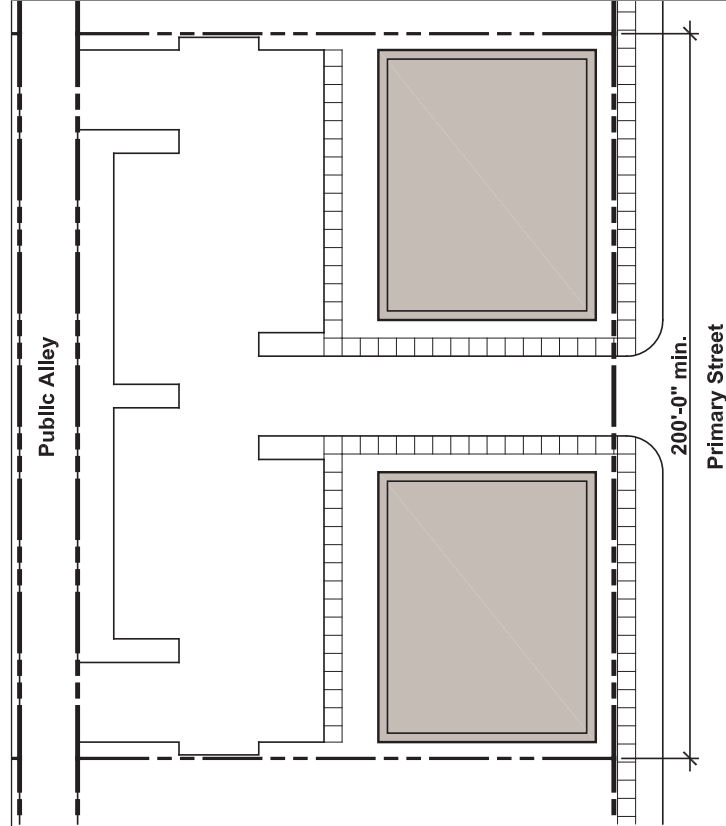


Figure 8.11c Parking Location/Access

LOT FRONTAGE EQUAL OR EXCEEDS 200'



c. Building design guidelines

Buildings should be designed to create an attractive and vibrant street environment and maintain a safe and comfortable pedestrian sidewalk environment (see figures 8.12a through 8.12d).

(1) Building massing

- (a) Building massing should be concentrated toward the primary public street or prominent public amenities, such as open spaces, plazas, or landscaped areas. On multistory buildings, upper stories may be set back to reduce the “canyon” effect and preserve view corridors.
- (b) Building massing should be used to define public open spaces and activate them through uses that engage the open space.
- (c) Upper-story massing should relate to overall and ground floor architectural elements in terms of spacing and rhythm.

- (d) Upper-story elements, such as balconies and window bays, should be used to create texture and avoid large blank surfaces as seen from a distance.
- (e) Adjacent to property lines shared with single-family attached or detached residential lots, building scale should be reduced.
- (f) Prominent corners should incorporate architectural massing such as turrets, towers, or distinct forms that provide terminal vistas and high-visibility locations for building entry.
- (g) Varied roof forms and elevations should be used to create interesting building silhouettes and avoid monotonous development forms.
- (h) Façade elements, such as structural members, planar variations, and/or material changes, shall be used to avoid expansive blank surfaces and create an appropriate sidewalk rhythm.

Figure 8.12 Desired Building Massing



a

Prominent corner massing and upper-story setback.



b

Upper-floor façade elements relate to ground-floor façade design.



c

Materials and façade elements prevent blank surfaces.



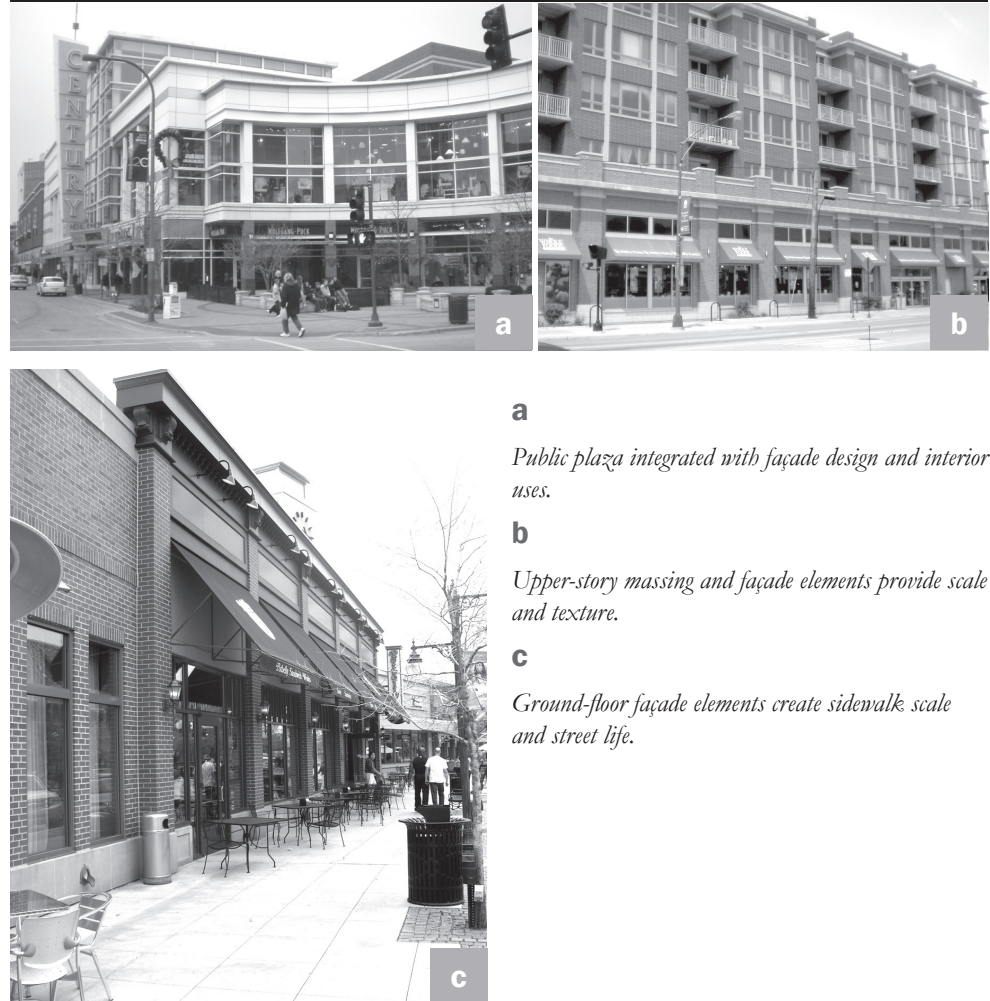
d

Varied façade design and roof form add interest.

- (2) Sidewalk environment (see figures 8.13a through 8.13c).
- (a) Landscaping shall be provided in accordance with the *Prince George's County Landscape Manual* for all yards and parking lots.
 - (b) Internal streets shall be designed and streetscaped in accordance with Section 5—Public Realm Standards of this overlay ordinance.
 - (c) Plazas, open spaces, public art areas, and other public amenities should be thoughtfully integrated into the overall character of the area and its surrounding buildings in terms of form, access, materials, and program.
 - (d) Commercial storefronts shall maintain an area equal to 60 percent of the front façade (measured between the ground plane and the top of the commercial storefront, not including upper façade sign friezes or extended parapets) for two-way transparency on the front façade.
 - (e) Ground-floor façades should use the following elements to create a comfortable and appropriately scaled pedestrian sidewalk environment:
 - i. Ground-plane kneewall
 - ii. Transparent commercial window area
 - iii. Ground-floor tenant entrance
 - iv. Upper-story tenant entrance
 - v. Commercial awnings with a sidewalk clearance between 7.5 feet and 8.5 feet
 - vi. Commercial signage frieze
 - vii. Decorative commercial cornice
 - (f) Upper-story façades should use the following elements to create an appropriate scale and relationship to the ground floor:
 - i. Upper-story massing with setbacks, planar variations, or structural articulation

- ii. Consistent fenestration design in terms of clustering, spacing, and proportion
- iii. Decorative eave line or upper-story parapet cornice

Figure 8.13 Desirable Façade Elements



(3) Style and detail

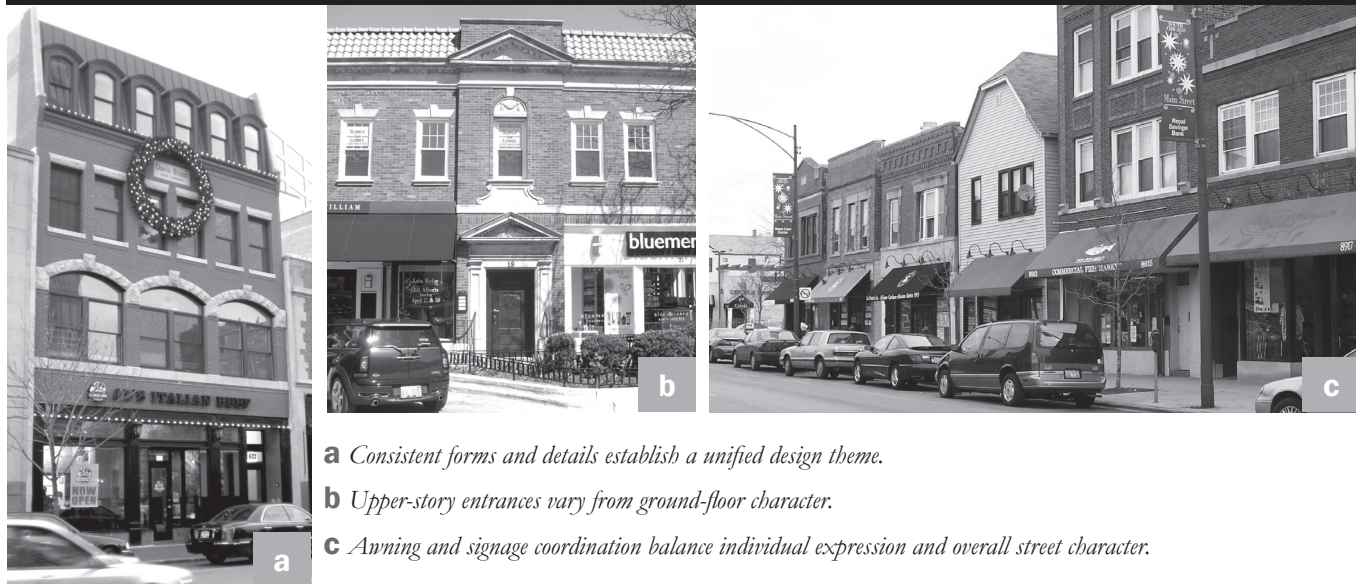
Commercial buildings should use façade details to create a specific design theme and aesthetic, especially in multiple-tenant or multiple-building shopping centers.

- (a) Building designs shall use materials with high aesthetic character, such as brick, decorative masonry, decorative metals, and decorative wood, to be determined through the design review process.
- (b) Low-quality materials, such as concrete masonry units, exterior insulating finishing system, or prefabricated panels, shall be minimized and masked wherever possible.
- (c) Specific design elements, such as masonry details, architectural trim elements, column bases and capitals, roof brackets, lighting, and awning forms,

etc., should be used on both ground-floor and upper-story façades to create a unified theme (see figure 8.14a).

- (d) For multistory development with separate building entrances for ground floor and upper-story tenants, the upper-story building entrance should be articulated differently than the ground-floor building entrance and use materials and detail elements that relate to the upper-story façade (see figure 8.14b).
- (e) A coordinated awning and signage plan should be used on multitenant or multibuilding developments to allow an appropriate amount of variation within a consistent set of standards related to the placement, form, and size of awning and building signage (see figure 8.14c).

Figure 8.14 Desirable Building Materials and Details



a Consistent forms and details establish a unified design theme.

b Upper-story entrances vary from ground-floor character.

c Awning and signage coordination balance individual expression and overall street character.

(4) Focal intersections

The Glenridge Transit Village Area includes intersections identified as focal intersections. These locations serve as important image-defining nodes and should reflect a high level of design and character (see figures 8.15a to 8.15c).

- (a) Buildings should incorporate articulated corners that create visual interest, provide entry plazas, and/or establish pocket open spaces adjacent to the public sidewalk.
- (b) Articulated corners may extend up to 15 feet above the permitted building height.

(c) Corner elements and buildings on focal intersections should incorporate innovative design and building materials that create a vibrant sidewalk environment and a visual terminus from nearby blocks in the corridor.

(d) Buildings on focal intersections should maximize façade transparency in order to create a more vibrant relationship between the public sidewalk, corner plazas, and interior uses.

Figure 8.15 Focal Intersection Design Principles



Massing, materials, transparency and details help establish neighborhood centers at key focal nodes.

2. Existing Residential Neighborhood

- a. Table 8.7 summarizes bulk and yard requirements for the **Existing Residential Neighborhood**. Figure 8.16 illustrates the regulations as described in the table.

Table 8.7 Existing Residential Area Bulk Table

	PRIMARY FRONTAGE TYPE	RESIDENTIAL ARTERIAL	LOCAL RESIDENTIAL STREET
Front Building Placement Line			
(A)	Minimum	100'*	20'
(B)	Maximum	110'*	30'
Corner Side Yard			
(C)	Minimum	10'	
Interior Side Yard			
(D)	Minimum	10'	
Rear Yard			
(E)	Minimum	10'	
Building Height			
	Maximum	35'	

*Note: Front Building Placement Lines for residential arterial frontages shall be measured from the Annapolis Road centerline. All other setbacks are to be measured from corresponding parcel lines.

b. Parking and access management

Parking access and location in this area reflect the goals of preserving the area's residential character.

- (1) For residential development (see figure 8.17a):
 - (a) The front façade of any garage, attached to or detached from the primary structure, shall be set back from the primary building façade at least 20 feet.
 - (b) One curb cut is permitted for each detached single-family development. For corner lots, such curb cut access shall be provided from the secondary street.

Figure 8.16 Residential Arterial & Local Residential Street Bulk Diagram

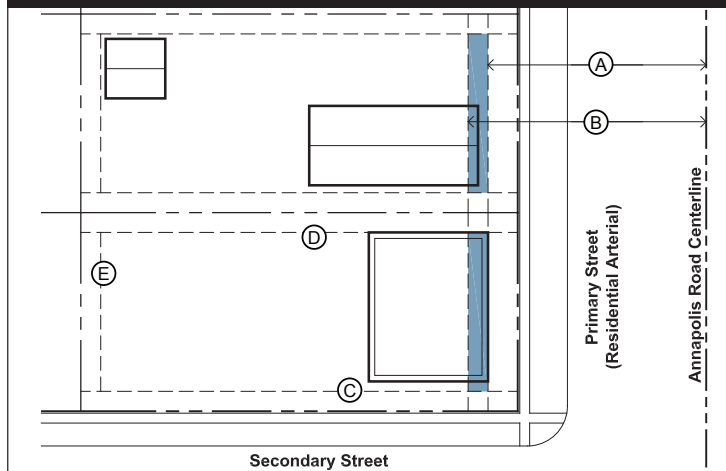


Figure 8.17a Residential Parking Location/Access

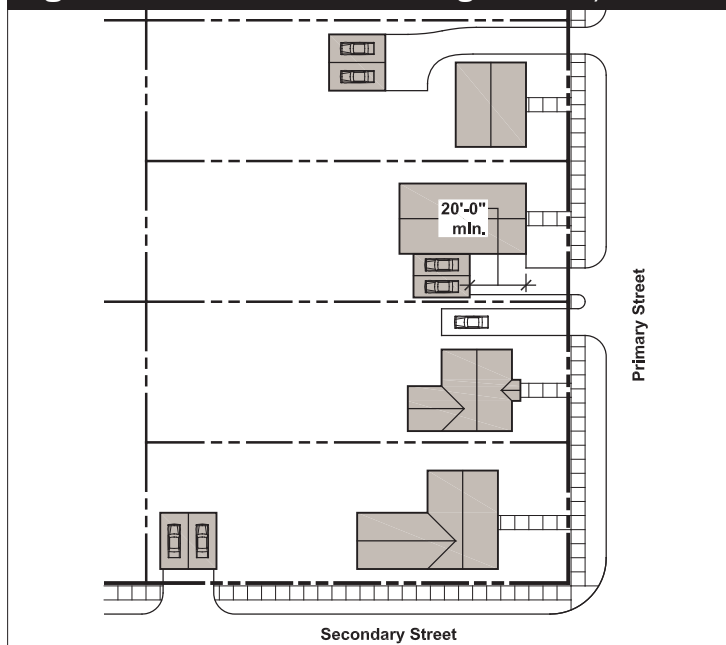
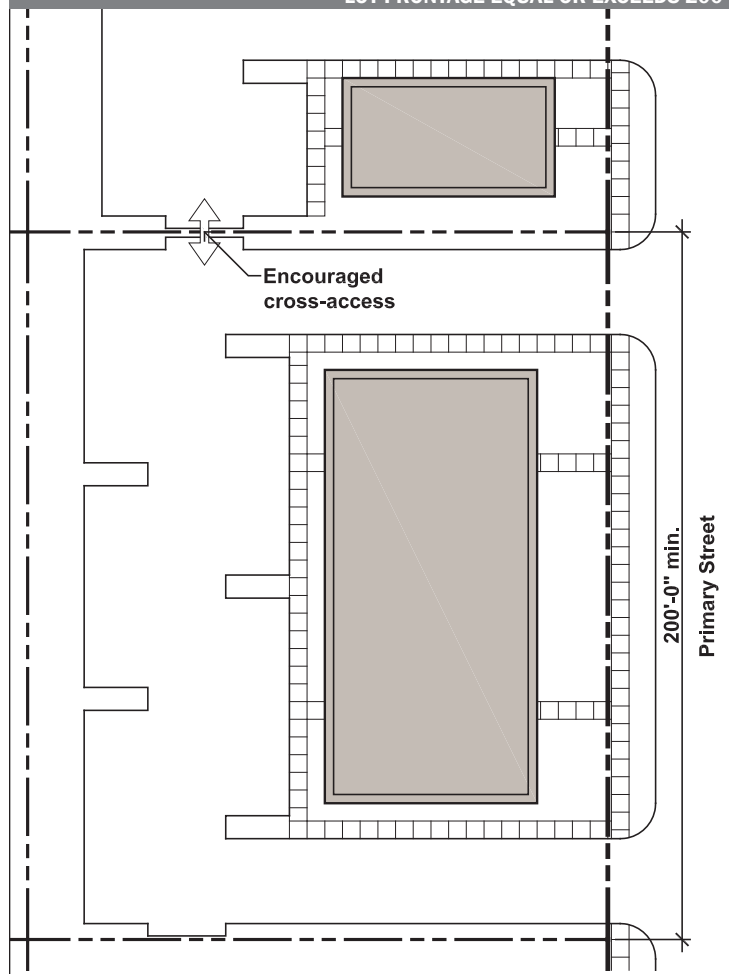


Figure 8.17b Commercial Parking Location/Access

LOT FRONTAGE EQUAL OR EXCEEDS 200'



- (c) For attached single-family development, curb cuts should be consolidated to the greatest extent possible by having attached units share access. For corner lots, such curb cut access shall be provided from the secondary street.
- (2) For commercial development (see figure 8.17b):
 - (a) No parking shall be located in between the primary building and the front or corner side yard lot lines.
 - (b) To the extent possible, parking should be located to the rear of the primary building.
 - (c) One double-loaded parking aisle may be located in one interior side yard, provided it is landscaped and screened in accordance with the *Prince George's County Landscape Manual*.
 - (d) For lots with less than 200 feet of frontage, one curb cut shall be permitted on the primary street.
 - (e) For lots with equal or exceeds 200 feet of frontage, two curb cuts shall be permitted on the primary street.
- (3) The following minimum and maximum parking requirements apply to the Existing Residential Neighborhood Area:
 - (a) For residential uses, the minimum required on-site parking capacity shall be 50 percent of the current required minimum capacity as determined in Section 27-568(a). The maximum permitted capacity shall be 1.5 spaces per dwelling unit.
 - (b) For commercial uses, the minimum required on-site parking capacity shall be 50 percent of the current required minimum capacity as determined in Section 27-568(a). The permitted maximum on-site capacity shall be equal to the minimum capacity typically required for all uses.

c. Building design guidelines

Development in this area should reflect the residential character of existing development in terms of bulk, scale, and general form (see figures 8.18a and 8.18b).

(1) Massing

- (a) For residential development, general massing elements, such as roof forms, dormers, window bays and porches, should reflect existing neighborhood development.
- (b) For commercial development, general massing elements such as storefront columns, party walls, and window bays should reflect the scale and rhythm of the surrounding residential character.

(c) The front façade of commercial developments should include the following elements:

- i. Ground-level kneewall and/or landscaping
- ii. Commercial window area
- iii. Commercial entrance(s)
- iv. Awnings mounted between 7.5 feet and 8.5 feet above grade
- v. Signage frieze
- vi. Decorative cornice or parapet

Figure 8.18 Building Design Principles

a

Form, scale, materials, and details should preserve residential character.

b

Commercial façades should reinforce neighborhood scale and character.



(2) Sidewalk environment

Development should utilize landscaping and screening to clearly delineate private property and the public realm in lieu of zero-setback buildings (see figures 8.19a and 8.19b).

- (a) Residential development should use 3 feet–4 feet tall semi-opaque decorative fencing along the front and corner side yard property lines.
- (b) All development should use property edge landscaping to clearly delineate the public sidewalk.
- (c) Commercial development should use landscaping along on-site pedestrian paths in order to create attractive entrances for tenants and patrons.

(3) Style and detail

- (a) Residential developments should incorporate porches, dormers, and massing elements into the design of the primary or corner side yard façades, and these elements should correlate in terms of form, scale, and placement.
- (b) Ground-floor and upper-floor façades should correlate in terms of fenestration placement, proportion, and spacing.
- (c) A consistent design theme should be created through the appropriate use of design details, massing elements, and building materials.

Figure 8.19 Building Design Principles



a
Property-edge screening delineates the public sidewalk when buildings are set back.

b
Commercial landscaping acknowledges the residential context and character.

3. Mixed-Use Transition

- Table 8.8 summarizes bulk and yard requirements for the **Mixed-use Transition Area**. Figures 8.20a and 8.20b illustrate the regulations as described in the table.
- Parking and access management

Table 8.8 Mixed-use Transition Area Bulk Table

	PRIMARY FRONTAGE TYPE	MIXED-USE ARTERIAL	LOCAL MIXED-USE STREET
Front Building Placement Line			
	Minimum (A)	87'*	30'
	Maximum (B)	92' for buildings, with non-residential uses OR buildings on corner lots, otherwise 107'*	35' for buildings, with non-residential uses AND buildings on corner lots, otherwise 15'
Corner Side Yard			
	Minimum (C)	0'	
	Maximum (D)	5'	10' for buildings with non residential uses, otherwise 5'
Interior Side Yard			
	Minimum (E)	0', unless against a residential-only area, then 5'	
	Maximum (F)	Aggregate of both interior side yard setbacks not to exceed 30% of lot width	Aggregate of both interior side yard setbacks not to exceed 40% of lot width
Rear Yard			
	Minimum (G)	0', unless against a residential-only area, then 20'	
Building Height			
	Maximum	50'	
Ground-Floor Height			
	Minimum	12' for building with non-residential uses or buildings on a corner lot	

**Note: Front Building Placement Lines along the mixed-use arterial frontage shall be measured from the Annapolis Road centerline. All other required setbacks are to be measured from the corresponding parcel lines.*

Figure 8.20a Mixed-use Arterial Bulk Diagram

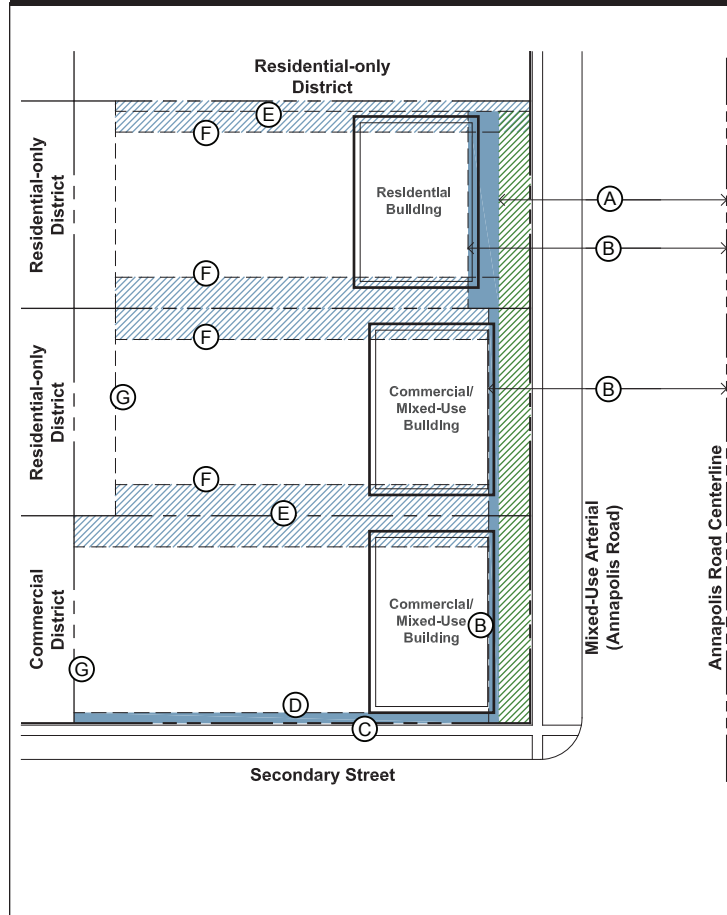
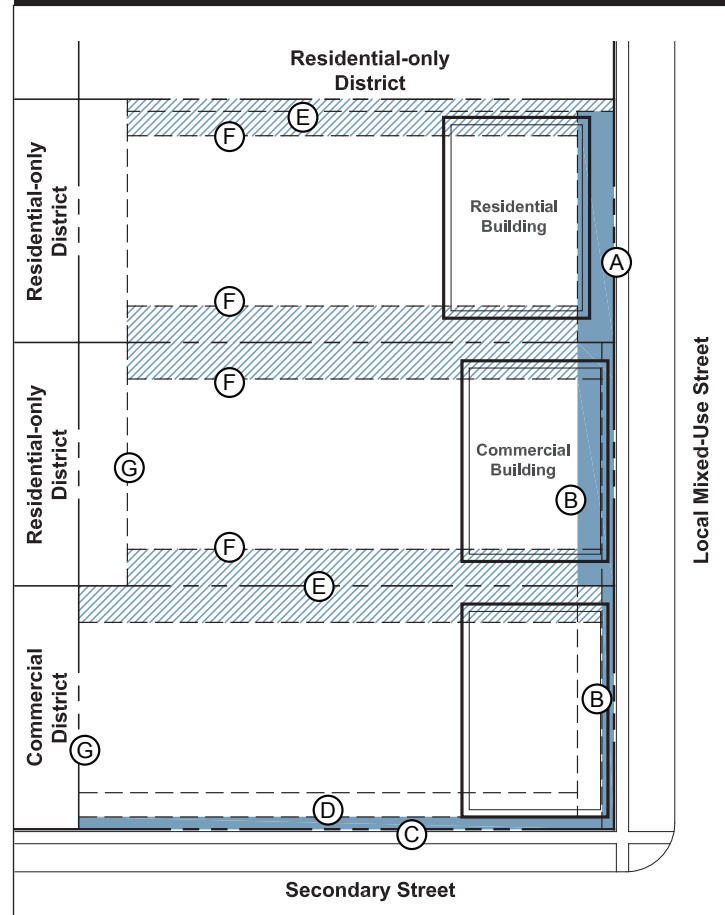


Figure 8.20b Local Mixed-use Street Bulk Diagram



Regulations in this area strive to balance automotive access and a strong pedestrian environment by encouraging minimal intermodal conflicts and comprehensive parking management.

- (1) No parking shall be located in the front yard or corner side yard.
- (2) For parking in the interior side yard, one double-loaded parking aisle is permitted for lots with 150 feet or more of frontage, provided the parking is set back from the primary building façade a minimum of 10 feet and is screened in accordance with the *Prince George's County Landscape Manual* (see figure 8.21a).
- (3) Direct pedestrian access from the primary public sidewalk shall be provided to each tenant via sidewalks and a front façade entry, or a sidewalk to a shared lobby that provides direct internal access to ground-floor tenants.
- (4) Curb cut access from the primary frontage street should be minimized wherever possible through shared curb cut access and cross-access between commercial tenants.
 - (a) For lots with less than 150 feet of frontage and no alley, one curb cut is permitted from a public street. On interior lots, this may be the primary street. On corner lots, a curb cut is only permitted from the side street (see figure 8.21a).
 - (b) For lots with 150 feet or more of frontage and no alley, one additional curb cut is permitted from the primary street above and beyond what is otherwise permitted.

- (c) For lots with access to a public alley and less than 100 feet of lot frontage, no curb cut from the primary street is permitted (see figure 8.21b).
 - (d) For lots with access to a public alley and 100 feet or more of lot frontage, one curb cut is permitted from the primary street (see figure 8.21b).
 - (e) For all lots with access to a public alley, access to parking shall be provided first from the alley, then from a side street or primary street only if necessary.
- (5) These minimum and maximum parking capacity regulations apply in the Mixed-Use Transition Area:
 - (a) For residential uses, the minimum required on-site parking capacity shall be 50 percent of the current required minimum capacity as determined in Section 27-568(a). The maximum permitted on-site capacity shall be 1.5 spaces per dwelling unit.
 - (b) For commercial uses, the minimum required on-site parking capacity shall be 50 percent of the current required minimum capacity as determined in Section 27-568(a). The permitted maximum capacity shall be equal to the required minimum capacity typically required for all uses.
- (6) To foster shared parking in this area, Section 27-570, Multiple Uses, and Section 27-572, Joint Use of a Parking Lot, shall be waived. The following regulations will apply instead.

Figure 8.21a Parking Location/Access

WITHOUT A PUBLIC ALLEY

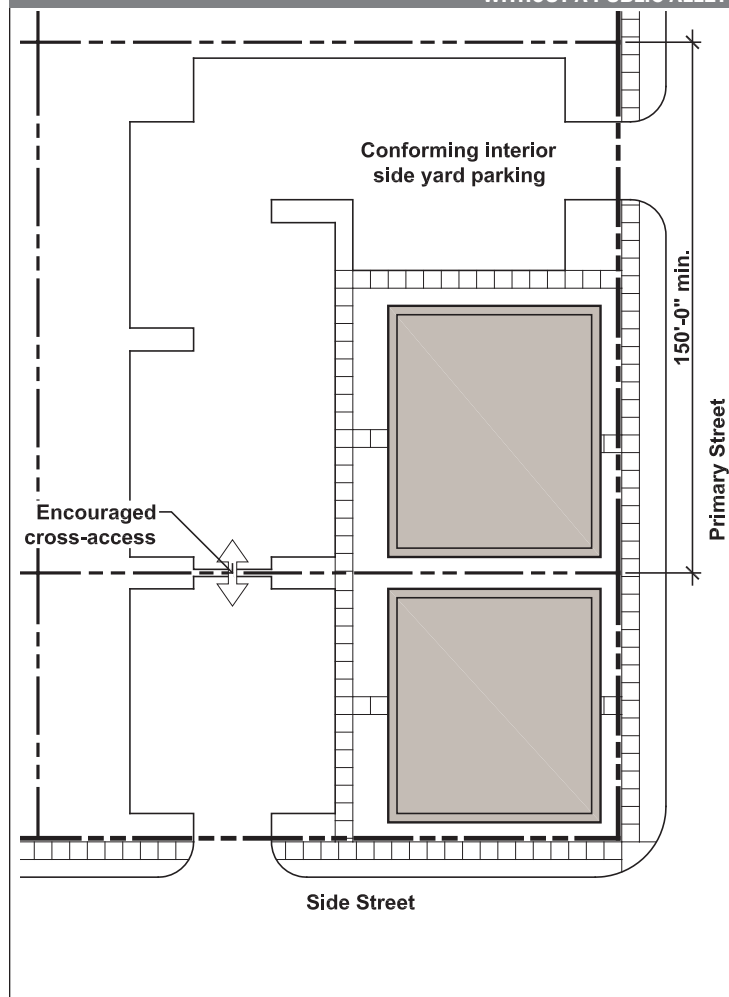
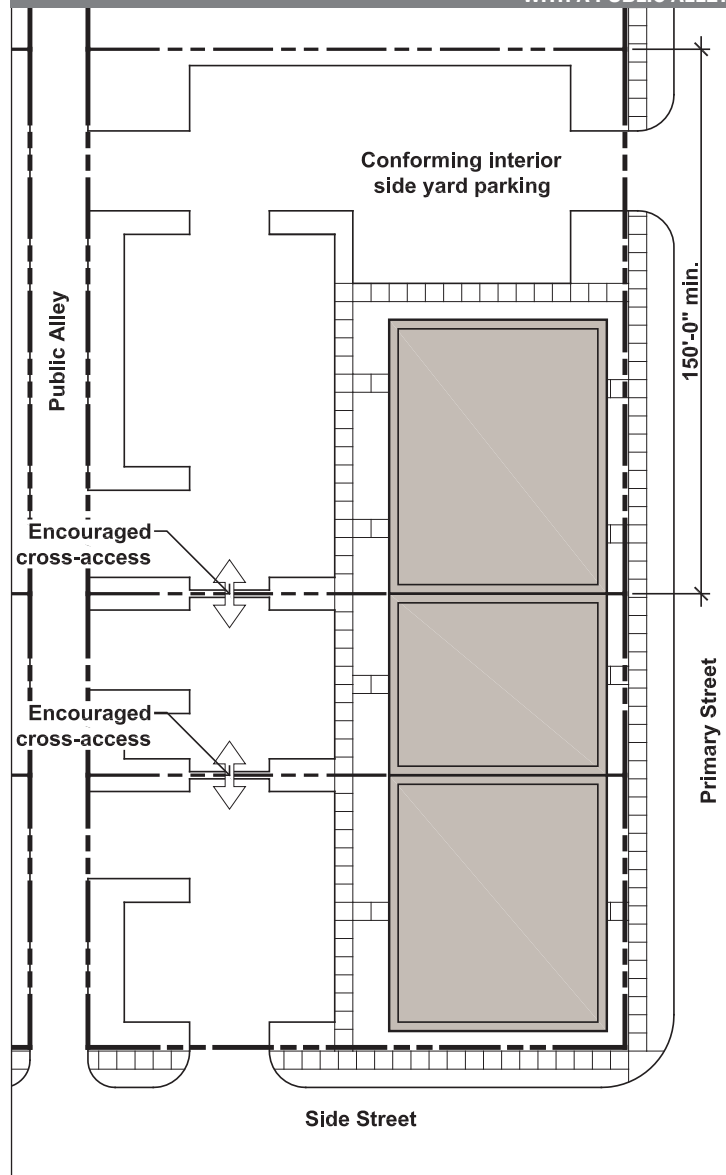


Figure 8.21b Parking Location/Access

WITH A PUBLIC ALLEY



- (a) For any property under one ownership and used for two or more uses, the number of spaces shall be computed by multiplying the minimum amount of parking required for each land use, as stated under section (5) above, by the appropriate percentage as shown in the shared parking requirements by time period (see Table 8.8). The number of spaces required for the development is then determined by adding the results in each column. The column totaling the highest number of parking spaces becomes the minimum off-street parking requirement.
- (b) For two or more uses under multiple ownership, the total off-street parking requirement may be satisfied by providing a joint parking facility, and the minimum requirements may be reduced in accordance with the procedure outlined in section (a) above for shared parking for single ownership. The Planning Board shall determine that shared parking is appropriate for the proposed uses and location if:

- i. The shared parking facility is within 500 linear feet, measured along the most appropriate walking routes between the shared parking facility and the entrances to all establishments being served.
- ii. The applicant provides a recorded shared-use parking agreement signed by all owners involved that ensures the shared parking facility will be permanently available to all current and future uses and also contains a provision for parking facility maintenance.

Table 8.8 Shared-Parking Reduction Percentage Multiplier

	WEEKDAY		WEEKEND		NIGHT
	6:00 A.M. - 6:00 P.M.	6:00 P.M. - MIDNIGHT	6:00 A.M. - 6:00 P.M.	6:00 P.M. - MIDNIGHT	MIDNIGHT - 6:00 A.M.
Office	100	10	10	5	5
Commercial/Retail	60	90	100	70	5
Restaurant	50	100	100	100	10
Lodging	70	100	70	100	70
R e c r e a t i o n a l / Entertainment/Social/ Cultural	40	100	80	100	10
Residential	60	90	80	90	100
Other	100	100	100	100	100

Source: Table based on "Shared Parking," a publication from the Urban Land Institute, Washington, D. C., 1983

c. Building design guidelines

Buildings should be designed to create an interesting shopping and living environment and to maintain a safe and comfortable pedestrian sidewalk environment (see figures 8.22a through 8.22d).

(1) Building massing

- (a) Building massing should be concentrated toward the primary public street. On multistory buildings, upper stories may be set back to reduce the “canyon” effect and preserve view corridors.
- (b) Upper-story massing should relate to overall and ground floor architectural elements in terms of spacing and rhythm.
- (c) Upper-story elements, such as balconies and window bays, should be used to create texture and avoid large blank surfaces when viewed from a distance.
- (d) Adjacent to property lines shared with single-family attached or detached residential lots, building scale should be reduced.
- (e) Prominent corners should incorporate architectural massing such as turrets, towers, or distinct forms that provide terminal vistas and high-visibility locations for building entry.
- (f) Varied roof forms and elevations should be used to create interesting building silhouettes and avoid monotonous development forms.
- (g) Façade elements, such as structural members, planar variations, and/or material changes, shall be used to avoid expansive blank surfaces and create an appropriate sidewalk rhythm.

Figure 8.22 Desired Building Massing



a
Prominent corner massing and upper-story setback.



b
Upper-floor façade elements relate to ground-floor façade design.



c
Materials and façade elements prevent blank surfaces.



d
Varied façade design and roof form add interest.

(2) Sidewalk environment

- (a) Landscaping shall be provided in accordance with the *Prince George's County Landscape Manual* for all yards and parking lots.
- (b) Internal streets shall be designed and streetscaped in accordance with Section 5—Public Realm Standards of this overlay ordinance.
- (c) Ground-floor residential facades should use an appropriate amount of transparency, varied materials, and design details to create texture and interest on the public sidewalk (see figure 8.23a).
- (d) Commercial storefronts shall maintain an area equal to 50 percent of the front façade (measured between the ground plane and the top of the commercial storefront, not including upper-façade sign friezes or extended parapets) for two-way transparency on the front façade. Ground-floor façades should use the following elements to create a comfortable and appropriately scaled pedestrian sidewalk environment (see figure 8.23b):

- i. Ground-plane kneewall
- ii. Transparent commercial window area
- iii. Ground-floor tenant entrance
- iv. Upper-story tenant entrance
- v. Commercial awnings with a sidewalk clearance between 7.5 feet and 8.5 feet
- vi. Commercial signage frieze
- vii. Decorative commercial cornice and/or parapet

- (e) Upper-story façades should use the following elements to create an appropriate scale and relationship to the ground floor (see figure 8.23c):
 - i. Upper-story massing with setbacks, planar variations, or structural articulation
 - ii. Consistent fenestration design in terms of clustering, spacing, and proportion
 - iii. Decorative eave line or upper-story parapet cornice

a

Residential ground-floor transparency enhances sidewalk vitality and safety.

b

Ground-floor façade elements create sidewalk scale and street life.

c

Upper-story massing and façade elements provide scale and texture.

Figure 8.23 Desirable Façade Design Elements

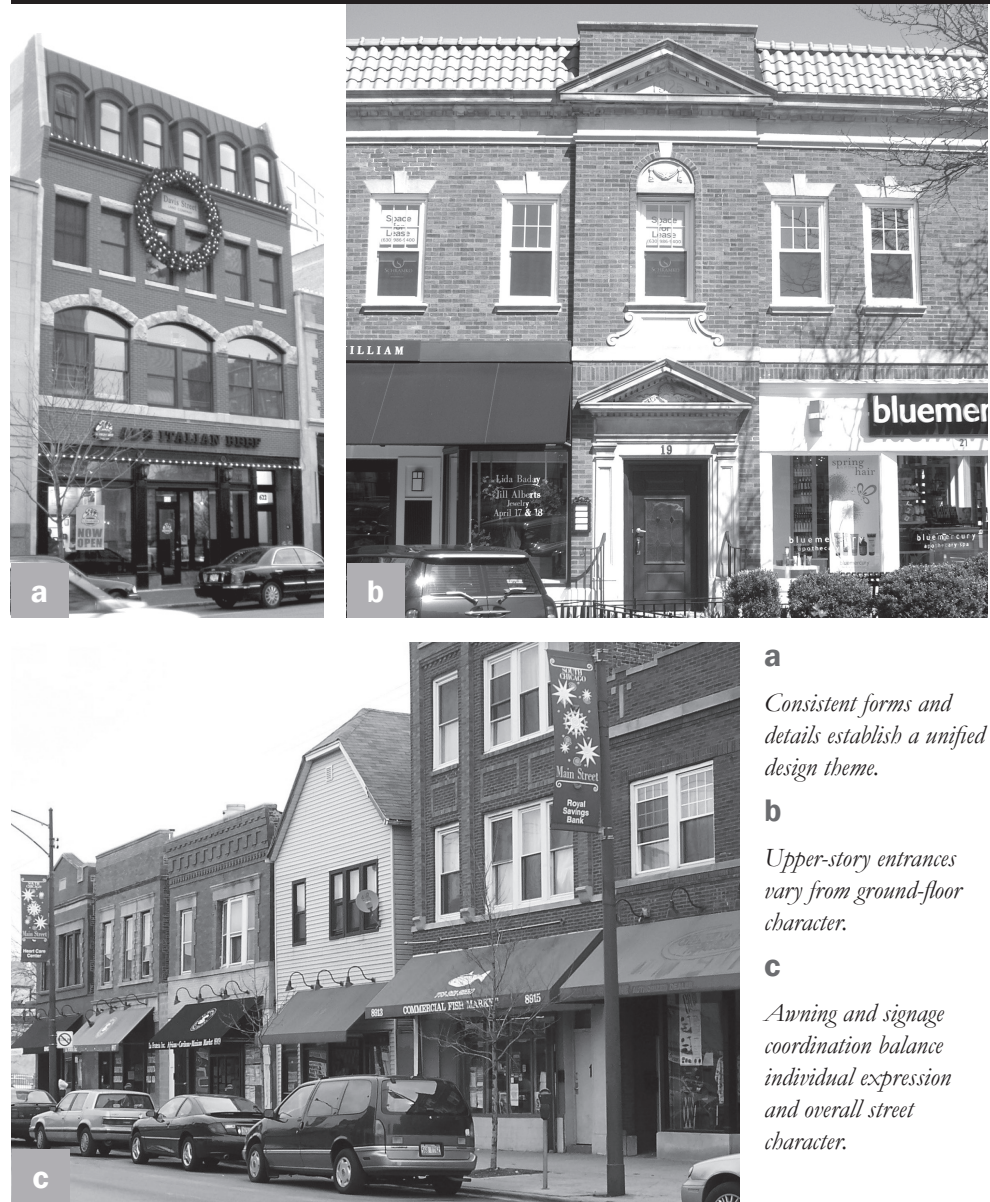


(3) Style and detail

Commercial and residential buildings should use façade details to create a specific design theme and aesthetic, especially in multiple-tenant or multiple-building developments (see figures 8.24a through 8.24c).

- (a) Building designs shall use materials with high aesthetic character, such as brick, decorative masonry, decorative metals, and decorative wood, to be determined through the design review process.
- (b) Low-quality materials, such as concrete masonry units, exterior insulating finishing system, or prefabricated panels, shall be minimized and masked wherever possible.
- (c) Specific design elements, such as masonry details, architectural trim elements, column bases and capitals, roof brackets, lighting and awning forms, etc., should be used on both ground-floor and upper-story façades to create a unified theme.
- (d) For multistory development with separate building entrances for ground-floor and upper story tenants, the upper-story entrance should be articulated differently than the ground-floor entrance, and it should use materials and detail elements that relate to the upper-story façade.

Figure 8.24 Desirable Building Materials and Details



- a**
Consistent forms and details establish a unified design theme.
- b**
Upper-story entrances vary from ground-floor character.
- c**
Awning and signage coordination balance individual expression and overall street character.

4. Retail Town Center

- a. Table 8.9 summarizes bulk and yard requirements for the Destination Shopping Center Area. Figures 8.25a and 8.25b illustrate the regulation as described in the table.

Table 8.9 Retail Town Center Bulk Table

	PRIMARY FRONTAGE TYPE	COMMERCIAL CORRIDOR ARTERIAL	COMMERCIAL PEDESTRIAN STREET
Front Building Placement Line*			
(A)	Minimum	60'**	0'
(B)	Maximum	110'	10'
Corner Side Yard			
(C)	Minimum	0'	
(D)	Maximum	N/A	10'
Interior Side Yard			
(E)	Minimum	0', unless against a residential-only area, then 10'	
Rear Yard			
(F)	Minimum	0', unless against a residential-only area, then 20'	
Building Height			
	Maximum	35'	
Ground-Floor Height			
	Minimum	12'	

* For development that faces both a Commercial Corridor Arterial and a Commercial Pedestrian Street that are intersecting or generally parallel, the commercial Pedestrian Street shall be deemed the primary-frontage street, making the Commercial Arterial the street along either the side property line or the rear property line. Where a rear property line runs along a Commercial Corridor Arterial, a 20-foot landscaped buffer with a double row of trees is required along that property line.

**Front Yard Building Line for the commercial corridor arterial frontage shall be measured from the Annapolis Road centerline. All other required setbacks are to be measured from the corresponding parcel line.

Figure 8.25a Retail Town Center Bulk Diagram

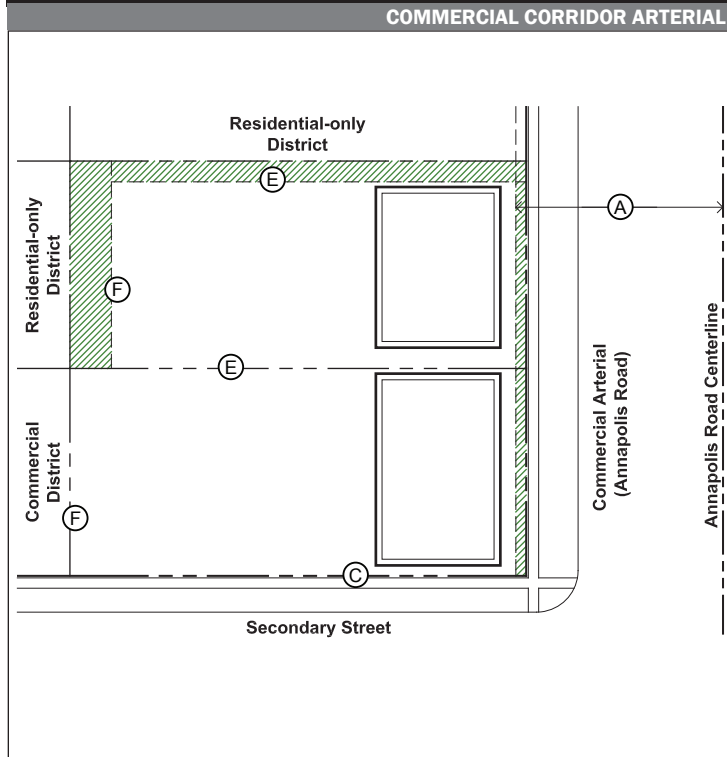
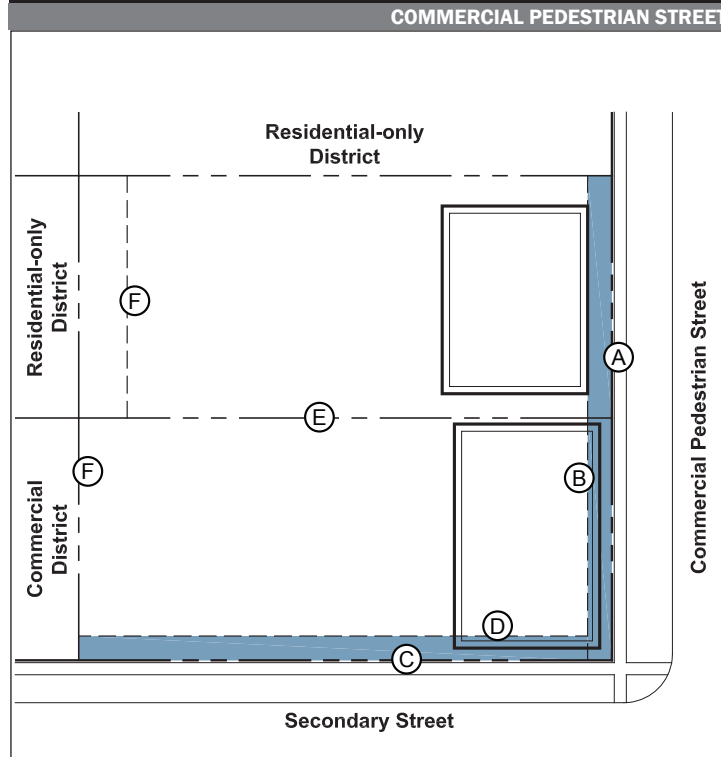


Figure 8.25b Retail Town Center Bulk Diagram



b. Parking and access management

Regulations in this section balance necessary automotive site access to commercial uses with the need to provide safe and attractive pedestrian and bike access to the same uses. They are also designed to minimize potential auto-related safety hazards to pedestrians and bicyclists.

(1) For development fronting on a Commercial Pedestrian Street (see figure 8.26a):

(a) No parking shall be located in the front yard or corner side yard.

i. For parking in the interior side yard, one double-loaded parking aisle is permitted for every 100 feet of building frontage. No more than one double-loaded parking aisle shall be located between two buildings, and it must be screened in accordance with the *Prince George's County Landscape Manual*.

ii. Direct pedestrian access from the public sidewalk on the Commercial Pedestrian Street shall be provided to each tenant via sidewalks and a front façade entry, or a sidewalk to a shared lobby that provides direct internal access to ground-floor tenants.

(b) Curb cut access from the Commercial Pedestrian and Commercial Arterial should be minimized wherever possible through shared curb cut access and cross-access between commercial tenants.

i. For lots with less than 200 feet of frontage, one curb cut is permitted from a public street. On interior lots, this may be the primary street. On corner lots, a curb cut is only permitted from the side street.

ii. For lots with 200 feet or more of frontage, one additional curb cut above and beyond what is permitted otherwise is permitted from the primary street (see figure 8.26b).

(c) Drive-through facilities should be located so that they are logically arranged within the on-site and contextual circulation plan. They should also be screened from the Commercial Pedestrian Streets and have minimal impact on pedestrian circulation and access.

(2) The following minimum and maximum parking capacity regulations apply to uses in the Retail Town Center Area: The minimum required on-site parking capacity for all uses shall be 50 percent of the current required minimum capacity as determined in Section 27-568(a). The maximum permitted on-site capacity shall be equal to the minimum capacity typically required for all uses.

(3) To foster shared parking in this area, Section 27-570, Multiple Uses, and Section 27-572, Joint Use of a Parking Lot, shall be waived. The following regulations will apply instead.

(a) For any property under one ownership and with two or more uses, the number of spaces shall be computed by multiplying the minimum amount of parking required for each land use, as stated under section (ii) above, by the appropriate shared-parking percentage by time period shown in Table 8.10. The number of spaces required for the development is then determined by adding the results in each column. The column totaling the highest number of parking spaces becomes the minimum off-street parking requirement.

(b) For two or more uses under multiple ownership, the total off-street parking requirement may be satisfied by providing a joint parking facility, and the minimum requirements may be reduced in accordance with the procedure outlined in section (a) above for shared parking for single ownership.

Figure 8.26a Parking Location/Access

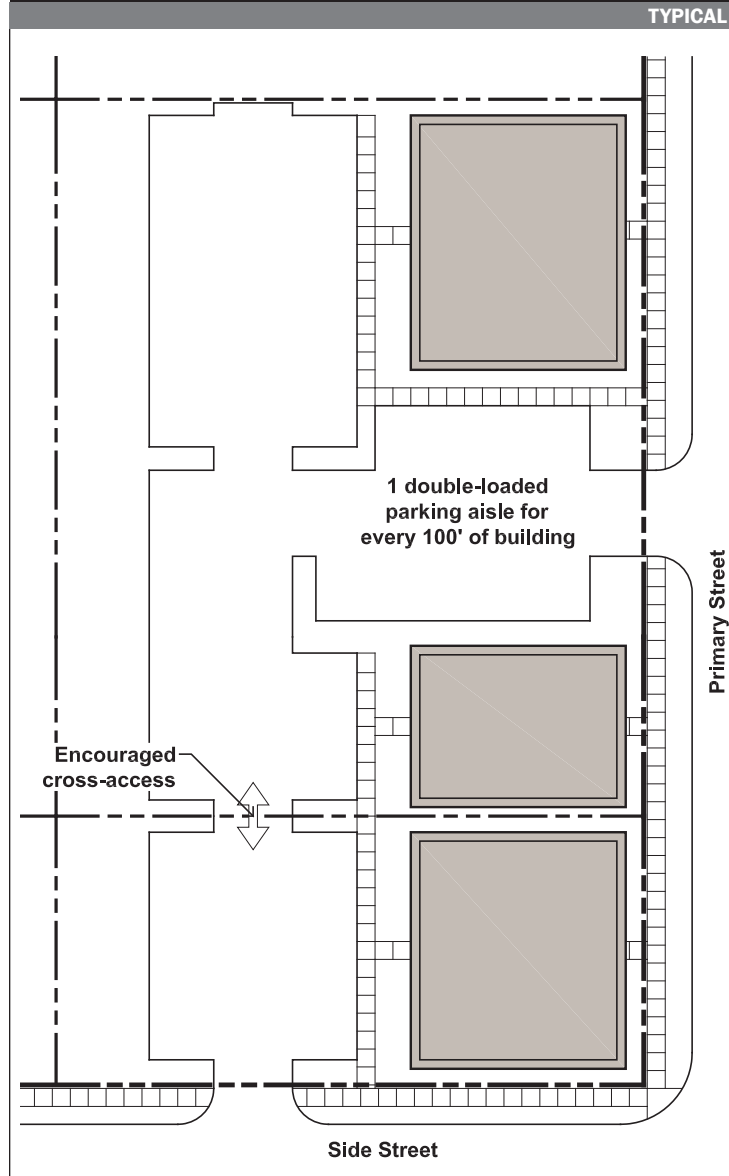
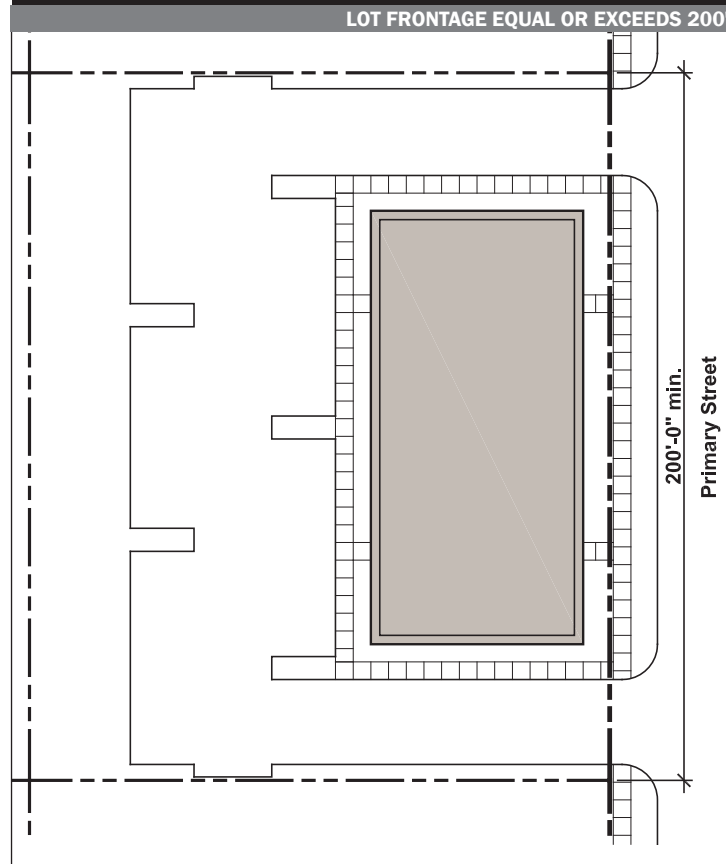


Figure 8.26b Parking Location/Access



The Planning Board shall determine that shared parking is appropriate for the proposed uses and location if:

- i. The shared parking facility is within 500 linear feet, measured along the most appropriate walking routes between the shared parking facility and the entrances to all establishments being served.
- ii. The applicant provides a recorded shared-use parking agreement signed by all owners involved which ensures the shared parking facility will be permanently available to all current and future uses and also contains a provision for parking facility maintenance.

Table 8.10 Shared-Parking Reduction Percentage Multiplier

	WEEKDAY		WEEKEND		NIGHT
	6:00 A.M. – 6:00 P.M.	6:00 P.M. – MIDNIGHT	6:00 A.M. – 6:00 P.M.	6:00 P.M. – MIDNIGHT	MIDNIGHT – 6:00 A.M.
Office	100	10	10	5	5
Commercial/Retail	60	90	100	70	5
Restaurant	50	100	100	100	10
Lodging	70	100	70	100	70
R e c r e a t i o n a l / Entertainment/Social/ Cultural	40	100	80	100	10
Residential	60	90	80	90	100
Other	100	100	100	100	100

Source: Table based on "Shared Parking," a publication from the Urban Land Institute, Washington, D. C., 1983

c. Building design guidelines

Buildings should be designed to create an interesting shopping environment and maintain a safe and comfortable pedestrian sidewalk environment (see figures 8.27a through 8.27d).

(1) Building massing

- (a) Building massing should be concentrated toward the primary public street.
- (b) Adjacent to property lines shared with single-family residential lots, building scale should be reduced.
- (c) Prominent corners should incorporate architectural massing such as turrets, towers, or distinct forms that provide terminal vistas and high-visibility locations for building entry.
- (d) Varied roof forms and elevations should be used to create interesting building silhouettes and avoid monotonous development forms.
- (e) Façade elements, such as structural members, planar variations, and/or material changes, shall be used to avoid expansive blank surfaces and create an appropriate sidewalk rhythm.

(2) Sidewalk environment

- (a) Landscaping shall be provided in accordance with the *Prince George's County Landscape Manual* for all yards and parking lots.
- (b) Internal streets shall be designed and streetscaped in accordance with Section 5—Public Realm Standards of this overlay ordinance.
- (c) Commercial storefronts shall maintain an area equal to 60 percent of the front façade (measured between the ground plane and the top of the commercial storefront, not including upper-façade sign friezes or extended parapets) for two-way transparency on the front façade.

- (d) Commercial storefronts should use the following façade elements to create a comfortable and appropriately scaled pedestrian sidewalk environment (figure 8.28):
- (i) Ground-plane kneewall
 - (ii) Transparent commercial window area
 - (iii) Sidewalk entrance

- (iv) Commercial awnings with a sidewalk clearance between 7.5 feet and 8.5 feet
- (v) Commercial signage frieze
- (vi) Decorative commercial cornice and parapet

Figure 8.27 Desired Building Massing



a. Buildings define the public sidewalk environment.

b. Massing on prominent corners creates interest from multiple views.

c. Varied façade and roof forms create interest and texture.

d. Materials, massing, and structural elements can create a pedestrian sidewalk scale.

Figure 8.28 Desirable Façade Design Elements



Ground-floor façade elements create sidewalk scale and street life.

(3) Style and detail

Commercial buildings should use façade details to create a specific design theme and aesthetic, especially in multiple-tenant or multiple-building shopping centers (see figures 8.29a and 8.29b).

(a) Building designs shall use materials with high aesthetic character, such as brick, decorative masonry, decorative metals, and decorative wood, to be determined through the design review process.

(b) Low-quality materials, such as concrete masonry units, exterior insulating finishing system, or prefabricated panels, shall be minimized and masked wherever possible.

(c) Specific design elements, such as masonry details, architectural trim elements, column bases and capitals, roof brackets, lighting and awning forms, etc., should be used to create a unified theme.

Figure 8.29 Building Design Principles

a

Form, massing, materials, and details create a unified theme and activate commercial streets.

b

Varied materials and formal details create an appropriate scale and interest.



5. Public Realm Standards

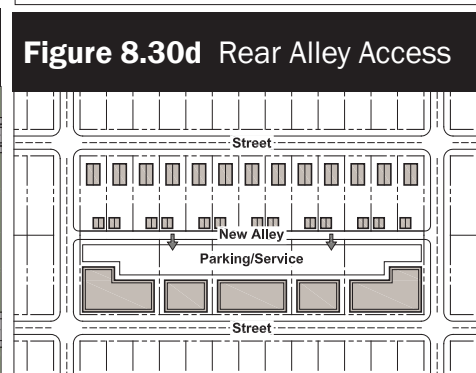
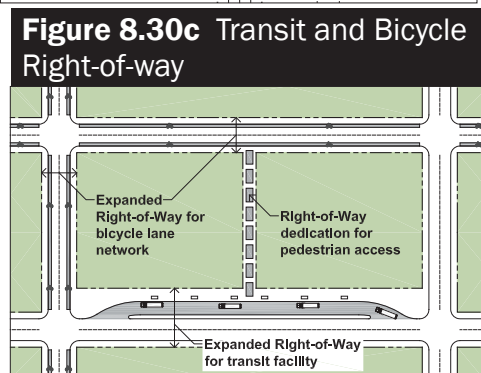
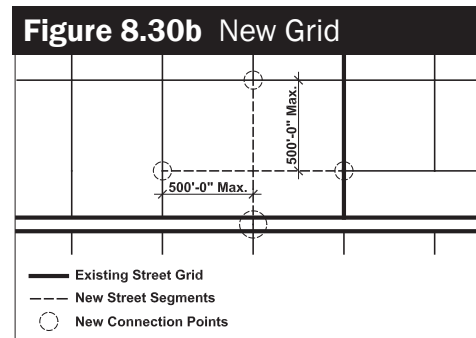
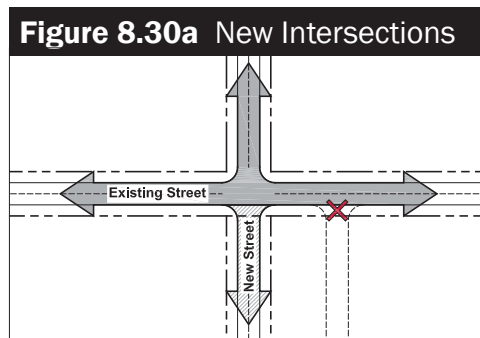
The following regulations apply to all new development in the Central Annapolis Road Development District Overlay Zone that involves the creation of new public streets, on-site traffic circulators, or frontage roads. The public realm should enhance functionality, access, and image by incorporating state-of-the-art planning and design concepts.

a. Street grid and blocks

The following regulations pertain to the establishment and placement of publicly accessible streets (see figures 8.30a through 8.30d).

- (1) New streets should serve as extensions of existing rights-of-way, and generally avoid awkward or unsafe intersection geometries.

- (2) Streets should form a grid that is generally consistent and integrated into the existing roadway network.
- (3) In areas intended for high levels of pedestrian activity, blocks should not be longer than 500 feet.
- (4) New blocks should incorporate public alleys to serve parking and service at the rear of development parcels.
- (5) Curb cuts should be minimized and provide access to interior parking areas that can be shared by several tenants or buildings.
- (6) Streets and blocks should accommodate multimodal amenities, such as dedicated walking and bicycle paths, and transit infrastructure and facilities.



a

New street segments should be linked to existing network nodes.

b

New streets should enhance the grid and multimodal accessibility.

c

Public alleys can provide parking and service access.

d

Transit, bicycle, and pedestrian infrastructure should be integrated into block design and right-of-way provisions.

Table 8.11 Required Right-of-way Width and Street Designation Key

Annapolis Road	(See Sections)
Pedestrian commercial street (minimum)	
> two-way, double-sided	65'
> one-way, single-sided	50'
Residential street (minimum)	60'
Public alley (minimum/maximum)	15'/20'



b. Street design

The following regulations summarize design requirements for new streets. For the purposes of this section, the following roadway definitions apply:

- *TOD Arterial* (7 lanes): 6 through lanes (3 in each direction), left-turn lane (see figures 8.33a)
 - *Mixed-use Transit Arterial* (9 lanes): 4 through lanes, left-turn lane, 2 service lanes, 2 parking lanes (see figures 8.33b)
 - *Mixed-use Arterial* (9 lanes): 4 through lanes, left-turn lane, 2 service lanes, 2 parking lanes (see figures 8.33c)
 - *Town Center Arterial* (7 lanes): 3 through lanes westbound, 2 through lanes eastbound, 1 service lane, 1 parking lane (see figures 8.33c)
 - *Commercial Corridor Arterial* (7 lanes): 3 through lanes westbound, 2 through lanes eastbound, 1 service lane, 1 parking lane (see figures 8.33d)
 - *Pedestrian commercial street*—a two-way street, other than Annapolis Road, that provides local access to private parcel frontage, typically on both sides of the street.
 - *Residential street*—a street, typically two-way, that hosts single-family attached or detached residential uses.
 - *Public alley*—a two-way drive, typically located between rear lot lines or rear building façades, that provides rear parcel access for parking and/or service.
- (1) Table 8.11 summarizes the required right-of-way widths for the roadway types defined above and identifies each type on a conceptual development plan (see figures 8.31a to 8.31c).
 - (2) Table 8.12 summarizes the required street elements and their widths for the roadway types defined above (see figures 8.31a through 8.31c):
 - (3) Table 8.13 summarizes optional street elements and their widths for the roadway types defined above (see figure 8.32).

Figure 8.31 Required Rights-of-way and cross section elements

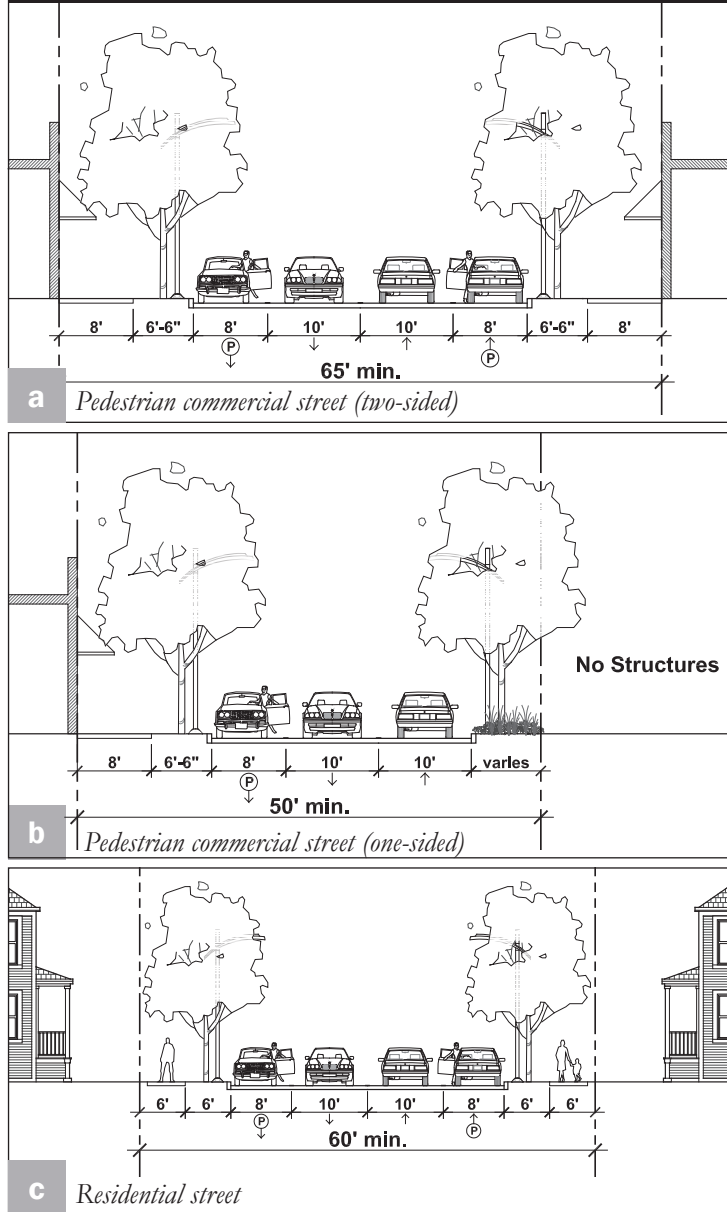


Table 8.12 Required Street Elements and Dimensions

STREET ELEMENT	ANNAPOLIS ROAD	PEDESTRIAN COMMERCIAL STREET	RESIDENTIAL STREET
Public sidewalk zone	8' min.	8' min.	6'
Furniture/planting zone (to face of curb)	6'-6"	6'-6"	6'
On-street parallel parking	8'	8'	8'
Vehicular travel lane/s (per lane)	11'	10'	10'

Table 8.13 Optional Street Elements and Dimensions

STREET ELEMENT	ANNAPOLIS ROAD	PEDESTRIAN COMMERCIAL STREET	RESIDENTIAL STREET
Café seating (in lieu of furniture zone)	8' min.	8' min.	N/A
On-street angled parking (in lieu of parallel parking)	18'	18'	N/A
Dedicated (on-street) bike lane	6'	6'	5'
Decorative median	6'	6'	6'

Figure 8.32 Optional Street Cross Section and Elements

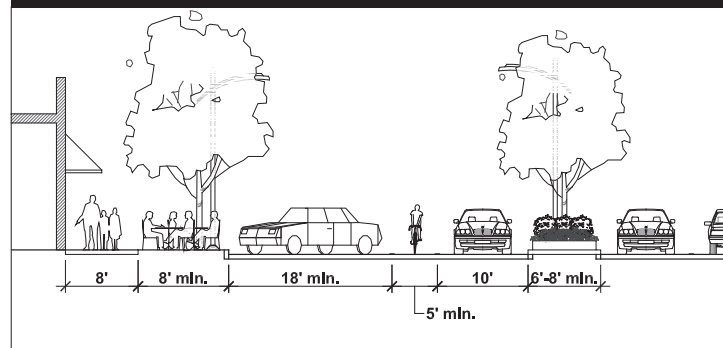


Figure 8.33a TOD Arterial

AT 'GLENRIDGE TRANSIT VILLAGE' (BETWEEN VETERANS PARKWAY AND GALLATIN ST)

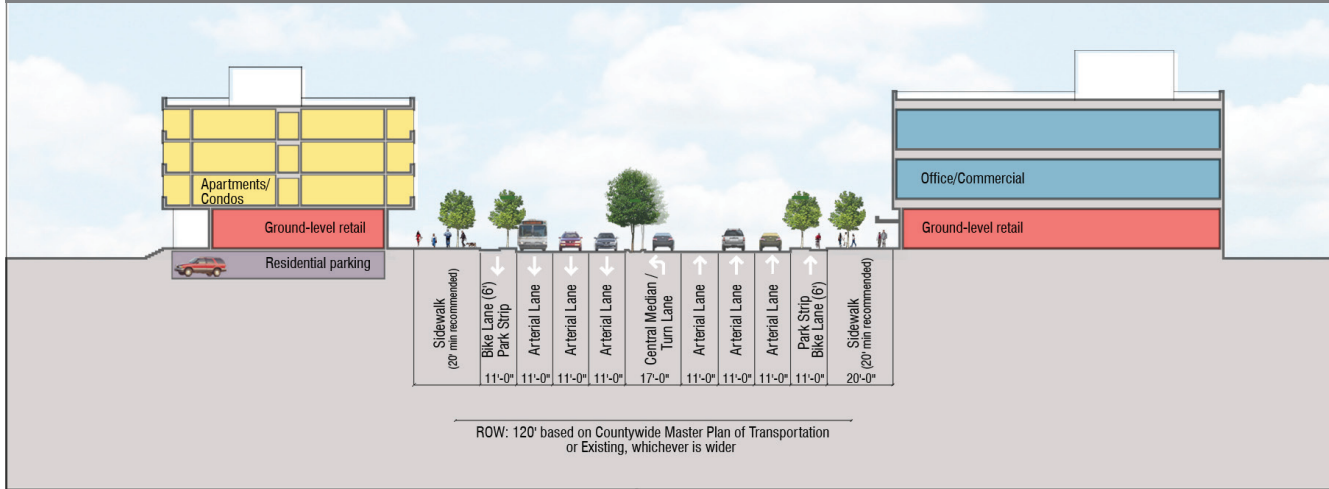


Figure 8.33b Mixed-use Transit Arterial

AT 'GLENRIDGE TRANSIT VILLAGE'(BETWEEN GALLATIN ST & ARDWICK ARDMORE/SURREY LN)

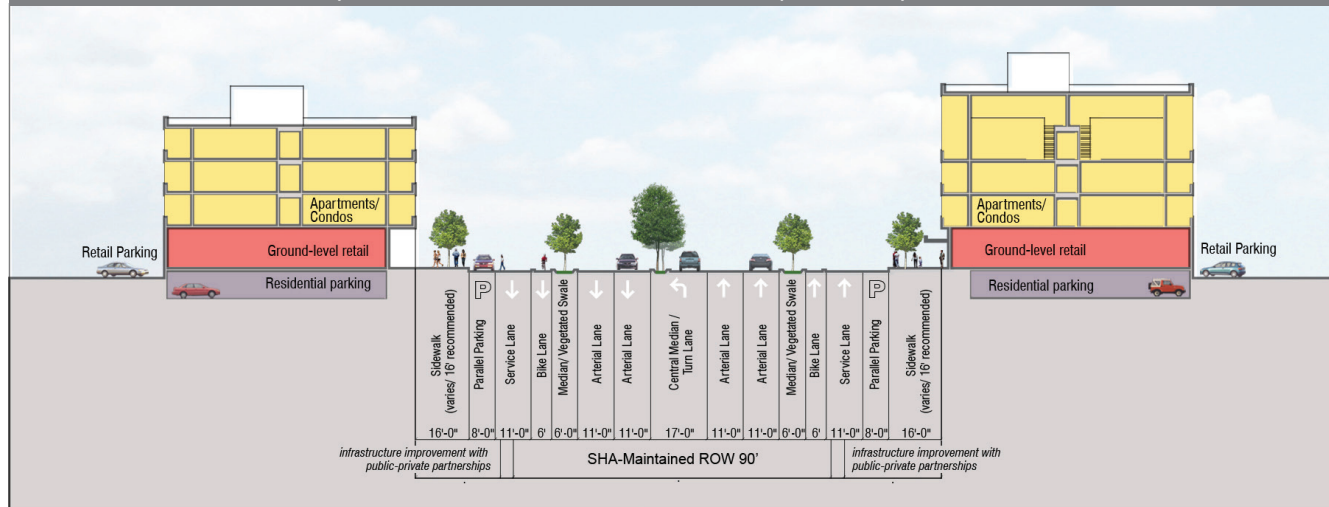


Figure 8.33c Mixed-use Arterial /Town Center Arterial

AT 'MIXED-USE TRANSITION AREA' AND 'RETAIL TOWN CENTER' (BETWEEN 68TH PLACE AND 65TH AVE.)

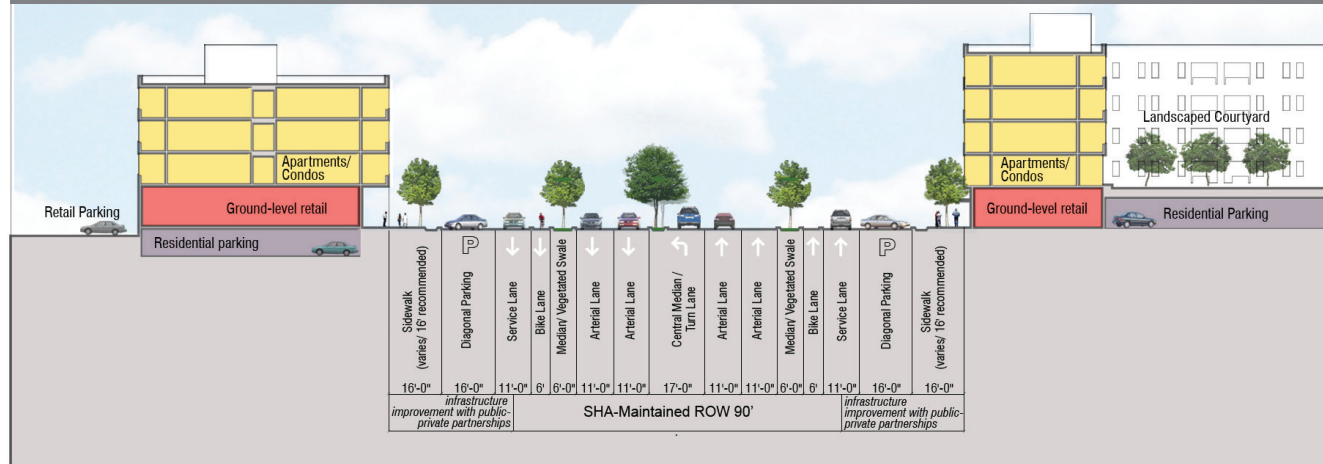


Figure 8.33d Commercial Corridor Arterial

AT 'RETAIL TOWN CENTER' (BETWEEN 65TH AVE. AND BALTIMORE-WASHINGTON PARKWAY)

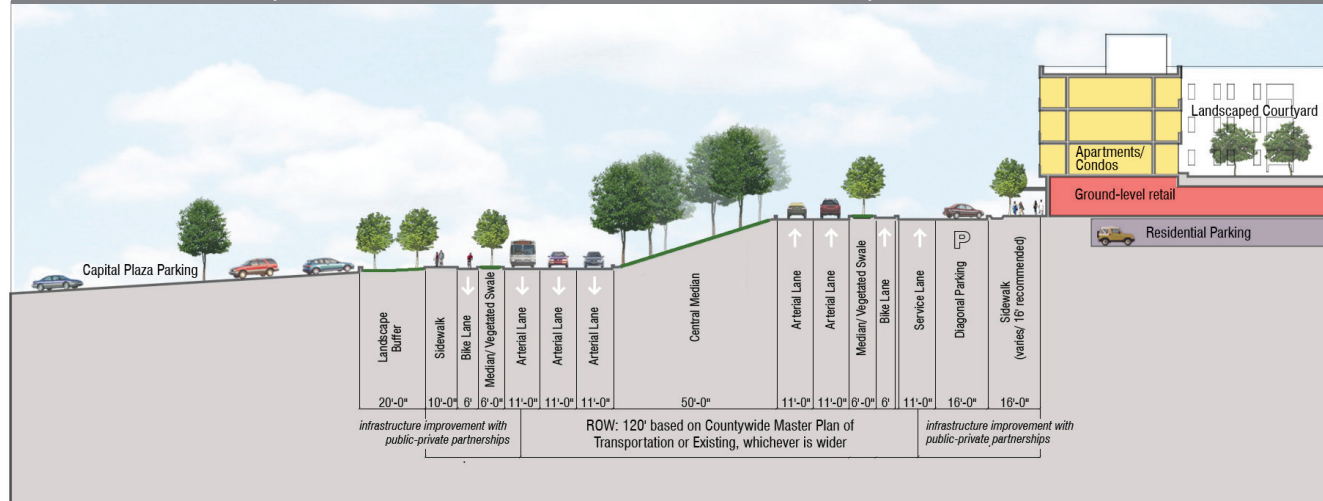


Figure 8.34 Public Space Design Principles



a Building mass is used to define the public plaza space.

b Hardscape and landscape elements create a comfortable and active space.

c Focal elements, lighting, and materials create texture and vibrancy that complement adjacent uses.

Figure 8.35 Bike/Pedestrian Infrastructure and Design



a Paths can provide a safe and comfortable mobility option.

b Paths can be integrated into street design.

c Private bicycle storage racks can enhance accessibility to commercial and residential areas.

c. Public spaces

The design of public spaces should relate to the surrounding built and natural context in terms of character, theme, and views (see figures 8.34a through 8.34c).

- (1) Buildings should maintain a direct relationship with public plazas by providing direct points of entry, facade transparency, and shared functions (i.e., outdoor seating for restaurants or cafes).
- (2) Public plazas shall include amenities that foster access, safety, and vitality, such as paths, benches, lighting, and visibility from private buildings and public thoroughfares.
- (3) Plazas are encouraged to include focal points of interest that relate to surrounding development, such as fountains, sculptures, or public gathering areas.
- (4) The massing of surrounding development shall not prohibit natural light access in essential portions of a plaza.
- (5) Adequate artificial lighting shall be provided to ensure a safe plaza environment.
- (6) Landscaping and hardscaping materials shall relate to surrounding development to the greatest extent possible.
- (7) Outdoor seating shall be located in the designated area between the public walkway and curb.
- (8) The placement of outdoor seating furniture and enclosures shall not interfere with use of the public walkway or on-street parking.

d. Transit, bicycle, and pedestrian mobility

Private development and the creation of new streets should enhance accessibility for pedestrians, bicyclists, and users of public transit (see figures 8.35a through 8.35c).

- (1) The following requirements relate to the accommodation of pedestrian and bicycle infrastructure and access:
 - (a) Development sites shall provide links to adjacent sidewalk or path networks to maintain continuity between development sites.
 - (b) The location of on-site path networks should maximize access to primary structures and minimize conflicts with automotive access and storage.
 - (c) Bicycle paths shall be no less than 8 feet wide.
 - (d) Paths that are not used to provide vehicular service or maintenance access are encouraged to use sustainable paving materials such as porous asphalt or permeable pavers.
 - (e) Paths shall be adequately illuminated and signed for safety and navigability.
- (2) The following requirements relate to the accommodation of transit operations and users (see figures 8.36a and 8.36b):
 - (a) To the extent possible, transit facilities shall be integrated into the planning and design of private development lots.
 - (b) Logical access and adequate pedestrian and vehicular circulation areas shall be provided around transit facilities.
 - (c) Transit facilities should link directly to the local pedestrian network.
 - (d) Transit facilities are encouraged to provide rider amenities such as bicycle racks and air-conditioned waiting areas.

Figure 8.36 Transit Infrastructure and Design



a Transit facilities should be integrated into destination uses.

b Transit stops should address pedestrian and bicycle networks and rider needs.

6. Landscape Standards

- a. Existing trees within the DDOZ should be preserved where feasible.**
- b. Residential uses with the DDOZ shall comply with the Residential planting requirements of the Landscape Manual.**
- c. Street Trees**

Street trees shall be provided along all streets to enhance and soften building façades, create street character, and provide shade for pedestrian street level activity. Street trees shall be planted at the time of development and spaced 30 feet apart on center. Where necessary, spacing allowances may be made to accommodate fire hydrants, utility vaults, overhead utility lines, and other infrastructure elements.

Street Trees (Arterial Frontage Road and Pedestrian Commercial Streets): Street trees shall be planted along the Arterial Frontage Road and all Pedestrian Commercial Streets in the DDOZ according to the streetscape sections. Street trees shall be a minimum three-inch caliper in size, located 30 feet on center, planted in tree pits (minimum five feet by ten feet), limbed up to six feet above finished grade, provide a minimum five cubic feet of continuous tree bed underneath the sidewalk pavement system, provide a positive drainage system, and provide an automated irrigation system to promote the health and vigor of the root system. Street trees species shall be large, broad spreading, open-canopy trees at maturity. Tree species that will not grow beyond 25 feet in height shall be planted underneath utility wires. If the utility wire is buried with the road construction, then larger tree species are recommended.

Street Trees (Residential Streets): Street trees shall be planted along all residential streets in the DDOZ according to the streetscape sections. Street trees shall be a minimum three-inch caliper in size, located 30 feet on center, planted in a continuous six-foot-wide minimum landscape strip or five feet by ten feet, provide a minimum five cubic feet of soil, and provide an automated irrigation system to promote the health and vigor of the root system. Street trees species shall be large, broad spreading, open-canopy trees at maturity. Tree species that will not grow beyond 25 feet in height shall be planted underneath utility wires.

DDOZ Street Tree Placement:

Street trees shall be planted along the street tree alignment within the streetscape and spaced at 30 feet on center.

d. Parking lots Requirements

Parking lots shall be screened from roadways and public areas (such as sidewalks, plazas, and abutting open space) with appropriate landscaping, a continuous, low masonry wall of three feet or less, or other appropriate screening techniques. Landscaping shall be provided in surface parking lots, as follows:

A landscaped strip consisting of a minimum four-foot-wide landscaped strip between the right-of-way line and the parking lot, with a brick, stone, or finished concrete wall between three and four feet in height shall be provided to screen the parking lot. The wall shall be located adjacent to but entirely outside the four-foot-wide landscaped strip. Plant with a minimum of one shade tree per 35 linear feet of frontage, excluding driveway openings, and with a mixture of evergreen groundcover and low shrubs planted between the shade trees. Parking Lot Interior Planting
Interior planting shall be required for any parking lot which

is 6,000 square feet or larger. A minimum of nine percent of the lot must be interior planting area. For purposes of calculation, all areas within the perimeter of the parking lot shall be counted, including planting islands, curbed areas, corner areas, parking spaces, and all interior driveways and aisles except those with no parking spaces located on either side. Landscaped areas situated outside the parking lot, such as peripheral areas and areas surrounding buildings, may not be counted as interior planting area.

In all parking lots, one shade tree per every ten spaces should be provided in corners, bump outs or islands.

If a parking lot less than 6,000 square feet is built without interior landscaping and later, additional spaces are added so that the total size of the lot is greater than 6,000 square feet, then the interior landscaping shall be provided for the entire parking lot.

Planting spaces must be large enough to allow for healthy tree growth and must be protected from parking or exiting vehicles, vehicle overhangs, and opening vehicle doors.

A minimum of 60 square feet of continuous pervious land area shall be provided for each tree. No tree planting area shall be less than five feet wide in any dimension.

A curb or wheelstop shall be provided for all parking spaces adjacent to planting or pedestrian areas to protect those areas from overhanging by parked vehicles.

Planting islands located parallel to parking spaces shall be a minimum of nine feet wide to allow vehicle doors to swing open.

In cases where a planting island is perpendicular to parking spaces and the spaces head into the planting island on both sides, the island shall be a minimum of eight feet wide to allow for bumper overhangs. If parking spaces are located on only one side of such a planting island, the island shall be a minimum of six feet wide.

e. Screening Requirements

All development is subject to section 4.4 screening requirements of the Landscape Manual unless otherwise specified below:

HVAC equipment, telecommunications buildings and equipment rooms related to monopoles and telecommunications towers, and satellite dish antennas shall be hidden from public streets, walks, and from all adjacent property containing residential, commercial, and mixed-uses, either by locating such equipment upon a roof behind a parapet wall or other device, or by utilizing landscaping, buffer walls, or other methods to screen the equipment.

Dumpsters and storage, service, loading, and delivery areas shall be hidden from public streets, walks, and from all adjacent property containing residential, commercial, and mixed-uses by utilizing landscaping, buffer walls, or other methods to screen the equipment.

f. Buffering development from Streets

All residential development is subject to and shall meet the screening requirements of 4.6 of the Landscape Manual if any rear yard is oriented toward a public right-of-way.

g. Buffering Incompatible Uses

Buffer yards between any uses contained within a property of a mixed-use development shall not be required.

Perimeter landscaping from incompatible uses as defined in Section 4.7 of the Landscape Manual shall consist of a landscaped strip to be a minimum of four feet wide, with a minimum three-foot-high brick, stone, or finished concrete wall, and/or plantings to consist of one tree and three shrubs per 35 linear feet of parking lot perimeter adjacent to a property line.

If walls are constructed, they shall be located adjacent to but entirely outside the four-foot-wide landscaped strip and shall provide at least one passage with a minimum width of three feet per every 60 linear feet when the wall is adjacent to open space, a pedestrian path, public plaza, or other pedestrian-oriented space to facilitate pedestrian movement and foster connections between parking areas and nearby uses.

The bufferyard requirements within the development district may be reduced to facilitate a compact form of development compatible with the recommendations of the Urban Design chapter. The minimum bufferyard requirements (landscape yard) for incompatible uses in the Landscape Manual (Section 4.7) may be reduced by 50 percent. The number of plant units required per 100 linear feet of property line or right-of-way may also be reduced by 50 percent. A four-foot-high, opaque masonry wall or other opaque screening treatment shall be provided in conjunction with the reduced width of the bufferyard between office/retail/commercial uses and residential uses.

h. Streetscape Elements

Streetscape elements of street trees, street furniture, landscaping and planters, decorative paving, sculpture/artwork, and bus shelters shall be shown on all Landscape & Lighting Plans. All streetscape elements shall be required for all streets and shall include information of location, spacing, quantity, construction details, and method of illumination in accordance with the plan's recommended streetscape sections and public realm elements. Advertisements and other commercial signage shall be prohibited on all streetscape elements with the exceptions of bus shelter advertisements approved by the appropriate public transit authority (WMATA or The Bus) and appropriate transit service-related notices at other locations within the Central Annapolis Road public realm subject to the approval of DPW&T and the appropriate municipality.

Streetscape elements shall include:

- Street trees (located in tree pits or continuous planting strips along major streets and planting beds along residential streets). Street trees planted in pits or planting beds shall be interconnected under the paving to provide continuous soil area for tree roots. These pits or planting beds shall be no less than 5 feet in width/diameter in any direction.
- Street furniture (benches, trash receptacles, lighting, and bus shelters)
- Landscaping and planters
- Decorative paving
- Sculpture/artwork

No street furniture or public works of art, other than publicly maintained streetlights and street trees, shall be installed within public rights-of-way without the permission of DPW&T and/or SHA expressed through an executed memorandum of understanding (MOU) with the developer/applicant. All street furniture on private rights-of-way within the DDOZ shall be maintained by the property owner/developer.

drought tolerance, maintenance requirements, and tolerance of adverse urban conditions, and shall be coordinated with the appropriate agencies. Native, non-invasive tree species are strongly recommended. Different selections from the palette should be made for each major street to avoid planting a monoculture consisting of a single species or type of tree and potentially losing all the trees within a development to disease.

i. Streetscape Paving

All primary walkways shall be constructed using decorative paving materials. Crosswalks may be constructed with tinted and stamped asphalt. Sidewalk and crosswalk paving materials may include concrete.

j. Permitted Materials

Brick, precast pavers, Belgium block, or granite pavers are permitted materials. Samples of proposed paving materials shall be submitted with the detailed site plan for review and approval by M-NCPPC staff and county/municipal public space maintenance agencies.

k. Streetscape Construction

All streetscape improvements shall be completed prior to the issuance of use and occupancy permits for the first building. Construction of streetscape improvements shall not be phased.

l. Diversity of Tree Species

A limited tree palette consisting of shade trees should be selected for gateways and other roads. Plant selection for street trees shall consider shape of canopy, sun and shade tolerance, presence or absence of overhead utility lines,

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